

LEGEND

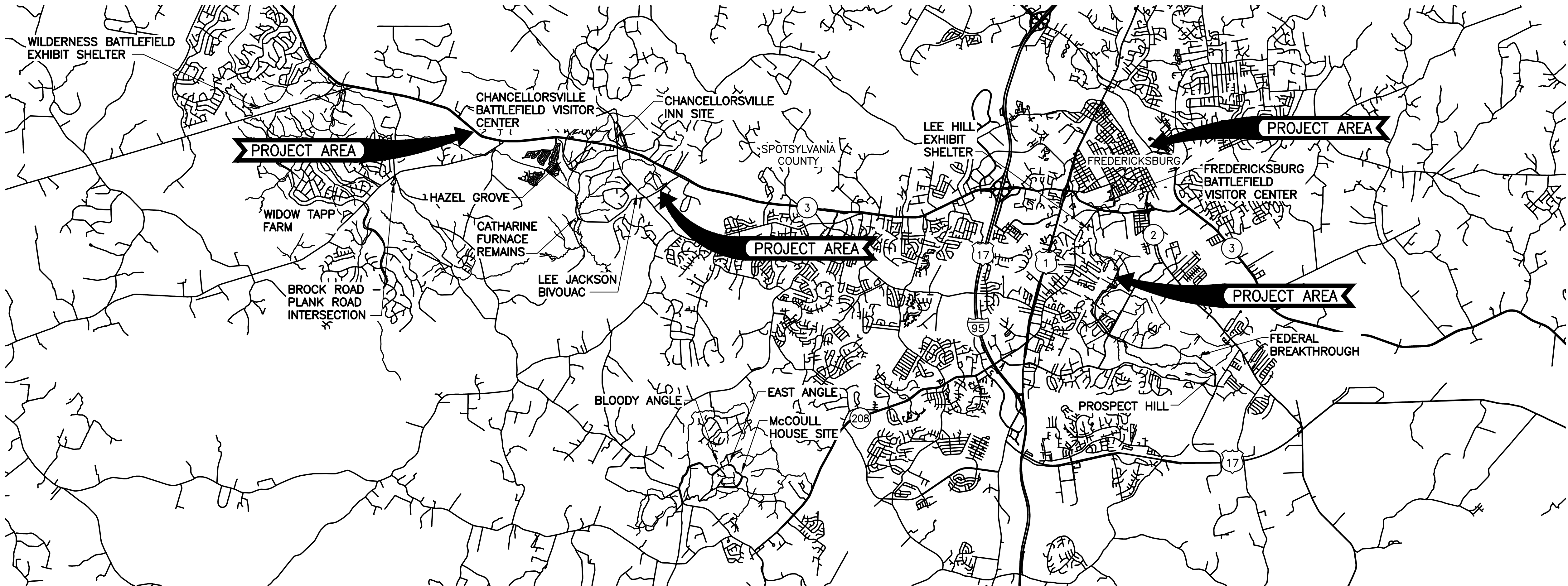
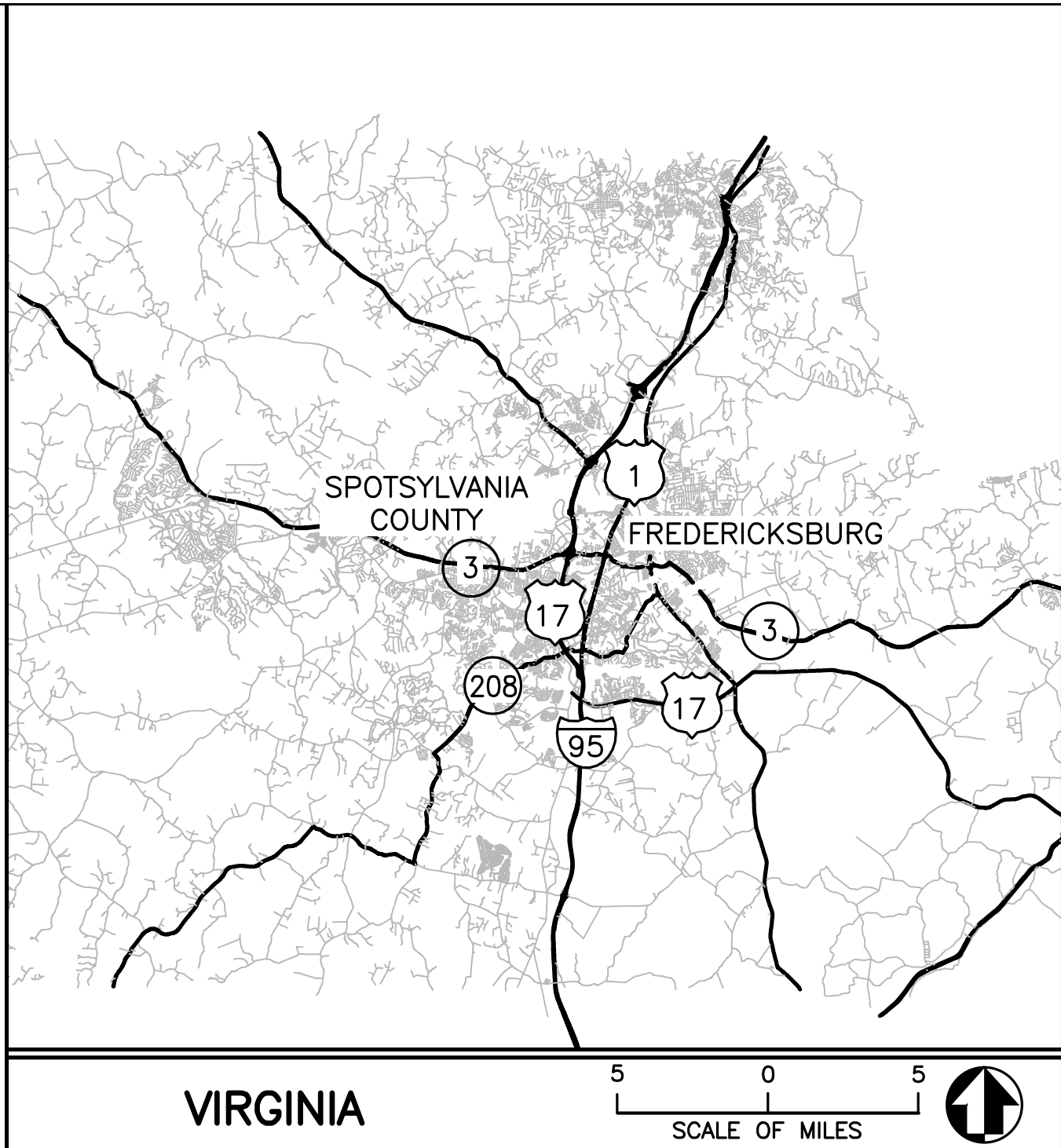
- PARK BOUNDARY
- STATE LINE
- COUNTY LINE
- ===== PAVED ROAD
- APPALACHIAN TRAIL
- .... CREEK OR RIVER

- V.C. VISITOR CENTER
- HQ. PARK HEADQUARTERS
- RANGER STATION
- TELEPHONE
- BOAT LAUNCH
- PARKING
- PICNIC AREA
- CAMPGROUND

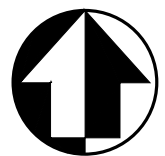
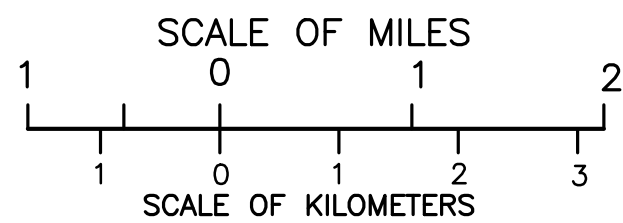
70% SUBMITTAL  
03-11-11

\* PROPOSED CONTRACT ADDITIONS \*

Note: 70% drawings to include work items beyond original scope. Further discussion required for additional contract items including pavement widenings, additional pavement locations, pavement markers, stop bars and chip seal.

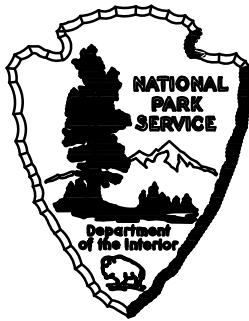


FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK



Mark	Sheet	REVISION	Date	Initial

QUALITY DESIGN CERTIFICATION	
<input type="checkbox"/> Prepared in Accordance with Design Development (Title I) _____ Drawing No. _____	<div>Project Manager</div> <div>Date</div>
OR	
<input type="checkbox"/> Variance from Design Development (Title I) _____ Approved by Superintendent on _____ Date _____	
OR	
<input type="checkbox"/> Construction Drawing Not Preceded by Design Development (Title I) _____	



UNITED STATES  
DEPARTMENT OF THE INTERIOR  
  
NATIONAL PARK SERVICE  
DENVER SERVICE CENTER

TITLE OF DRAWING  
PAVEMENT MANAGEMENT  
  
LOCATION WITHIN PARK  
VARIOUS  
  
NAME OF PARK  
FREDERICKSBURG AND SPOTSYLVANIA N.M.P.  
REGION COUNTY STATE  
NORTHEAST VARIOUS VIRGINIA

DRAWING NO. XXX	
PKG. NO.	SHEET
	1
	OF 23

3/11/11 11:43 ASCHAVE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\GEN\C1-INDEX.DWG XREFS: \\VHB\PROJ\VAEBACH\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\BASES\DATE.DWG;

SHEET INDEX

SHEET	SUBSHEET	TITLE OF SHEET
1		COVER
2	C1	INDEX AND GENERAL NOTES
3	C2	DETAILS
4	C2A	DETAILS
5	C2B	DETAILS
6	C3	MAINTENANCE OF TRAFFIC PLAN
7	C3A	MAINTENANCE OF TRAFFIC PLAN
8	C4	LEE DRIVE ROUTE 10
9	C4A	LEE DRIVE ROUTE 10
10	C5	PARKING AREA 1 ROUTE 905
11	C6	PARKING AREA 2 ROUTE 906
12	C7	RANGER HEADQUARTERS/VISITOR PARKING ROUTE 908A & 908B
13	C8	PARKING AREA 4 & PICKETT CIRCLE PARKING ROUTE 931 & 910A
14	C9	LEE DRIVE PARKING AREA 6 ROUTE 933
15	C10	PARKING AREA 5A & AREA 5B ROUTE 932A & 932B
16	C11	UNION BREAKTHROUGH
17	C12	LEE DRIVE PARKING AREA 3 ROUTE 907
18	C12A	LEE DRIVE PARKING AREA 3 ROUTE 907 PAVEMENT MARKING PLAN
19	C13	MCCLAW'S DRIVE ROUTE 13
20	C14	JACKSON'S FLANK ATTACK TURN LANE
21	C14A	JACKSON'S FLANK ATTACK TURN LANE PAVEMENT MARKING PLAN
22	C14B	JACKSON'S FLANK ATTACH MAINTENANCE OF TRAFFIC DETAILS
23	C15	CHATHAM MANOR DRIVEWAY

\* PROPOSED CONTRACT ADDITIONS \*

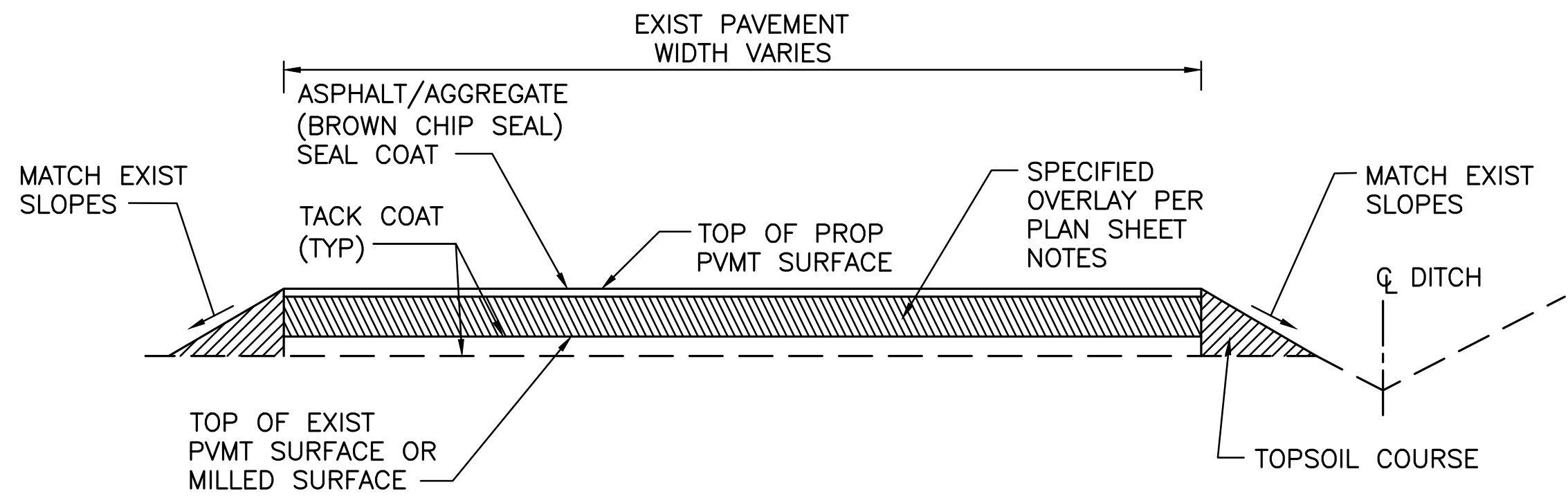
Note: 70% drawings to include work items beyond original scope. Further discussion required for additional contract items including pavement widenings, additional pavement locations, pavement markers, stop bars and chip seal.

GENERAL NOTES

- PAVEMENT MARKINGS AND TRAFFIC FLOW ARROWS SHOWN ON THE ROADWAY CONSTRUCTION PLANS ARE SCHEMATIC ONLY. THE ACTUAL LOCATION AND APPLICATION OF PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH MUTCD, SEQUENCE OF CONSTRUCTION/ TRAFFIC CONTROL PLANS, PAVEMENT MARKING PLAN SHEETS. FINAL LAYOUT SHALL BE APPROVED BY THE C.O. PRIOR TO APPLYING MARKINGS OR SYMBOLS TO ANY PAVEMENT.
- ALL PAVEMENT MARKINGS SHALL BE FEDERAL TYPE B WATER-BORNE TRAFFIC PAINT WITH TYPE II GLASS BEADS, IN ACCORDANCE WITH THE SPECIFICATIONS.
- FOR THE TEMPORARY EROSION AND SILTATION CONTROL THE CONTRACTOR, IN CONJUNCTION WITH THE C.O. AND/OR ENVIRONMENTAL MONITOR, SHALL ADJUST THE LOCATION, QUANTITY AND TYPE OF EROSION AND SEDIMENT CONTROL ITEMS REQUIRED BASED ON THE ACTUAL FIELD CONDITIONS ENCOUNTERED AT THE TIME OF CONSTRUCTION AND THE ACTUAL SCHEDULING AND SEQUENCING OF THE CONSTRUCTION ACTIVITIES.
- THE AREAS BEYOND THE PROJECT'S CONSTRUCTION AREA ARE TO BE PROTECTED FROM SILTATION. PERIMETER CONTROLS SUCH AS FILTER BARRIER, SILT FENCE, DIVERSION DIKES, TURBIDITY CURTAINS, ETC. SHALL BE INSTALLED PRIOR TO ANY GRUBBING OPERATIONS OR OTHER EARTH MOVING ACTIVITIES.
- TEMPORARY EROSION AND SILTATION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK.
- ALL CONSTRUCTION VEHICLES AND EQUIPMENT ARE TO BE POWER WASHED PRIOR TO ARRIVAL TO PARK TO REMOVE SOIL AND SEEDS FROM PREVIOUS JOBS. POWER WASHING SHOULD INCLUDE CAREFUL ATTENTION TO UNDERCARRIAGE, TIRES, AND WHEEL WELLS, AS WELL AS THE EXTERIOR OF THE VEHICLE. VEHICLE CLEANLINESS WILL BE INSPECTED BY C.O. ON-SITE PRIOR TO THE VEHICLE BEING USED.
- SURVEY AND LIMITS OF PAVED AREAS SHOWN IN THE DRAWINGS ARE FOR GENERAL INFORMATION ONLY. NEW EDGE OF PAVEMENT IS INTENDED TO MATCH EXISTING EDGE OF PAVEMENT (UNLESS OTHERWISE NOTED) AND BE LAID TO PRODUCE SMOOTH RADII AND TANGENT SECTIONS ALONG THE EDGE. JAGGED OR IRREGULAR EDGES ON NEW PAVEMENT SHALL BE REJECTED AS INFERIOR WORK.
- PRIOR TO PLACEMENT OF SHIM AND LEVELING AND/OR OVERLAY PAVEMENT COURSES, ALL BOUND SURFACES (HMA AND/OR CHIP SEAL) SHALL BE CLEANED IN ACCORDANCE WITH THE SPECIFICATIONS. FOR EXISTING LOCATIONS WITHOUT A BOUND MATERIAL, PREPARATION OF THE SUBGRADE SHALL BE IN ACCORDANCE WITH THE FULL DEPTH PATCH REPAIR SPECIFICATIONS.

DESIGNED: CAL	SUB SHEET NO.  C1	TITLE OF SHEET  INDEX AND GENERAL NOTES  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____
TECH. REVIEW: TNR			PMIS/PKG NO. 147784
DATE: 03-11-11			SHEET 2 of 23
ALS			
ADD			

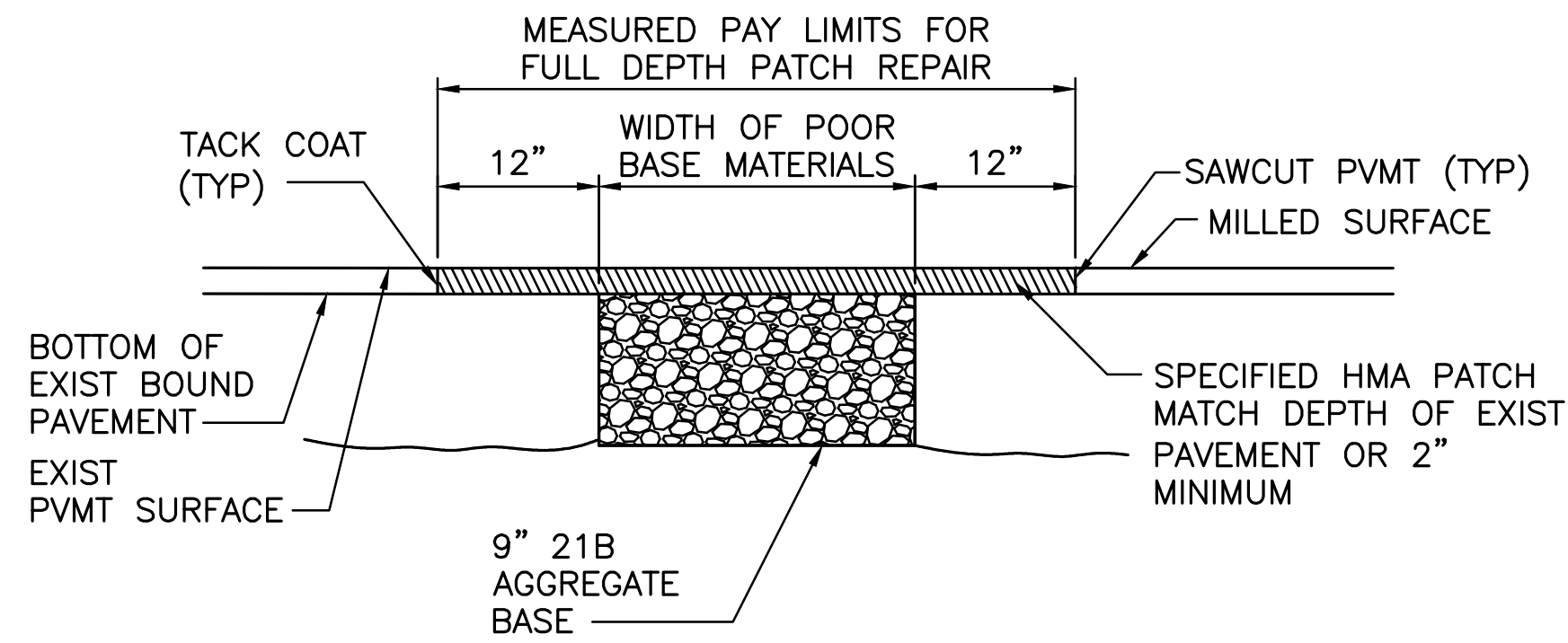
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NOTES:

1. PRIOR TO PLACEMENT OF THE OVERLAY COURSE, GRASS AND SEDIMENT SHALL BE REMOVED FROM THE EDGES OF PAVEMENT IN ORDER TO FULLY EXPOSE THE EDGES OF PAVEMENT.
2. TOPSOIL COURSE SHALL BE REQUIRED ADJACENT TO ALL OVERLAY AREAS WHERE THE FINAL PROPOSED PAVEMENT SURFACE ELEVATION IS GREATER THAN 1" TO TIE TO EXISTING GRADE.
3. ASPHALT/AGGREGATE (BROWN CHIP SEAL) SEAL COAT IS REQUIRED AT ALL LOCATIONS EXCLUDING MCLAWS DRIVE, FLANK ATTACK ROAD TURN LANE AND CHATHAM MANOR DRIVEWAY.

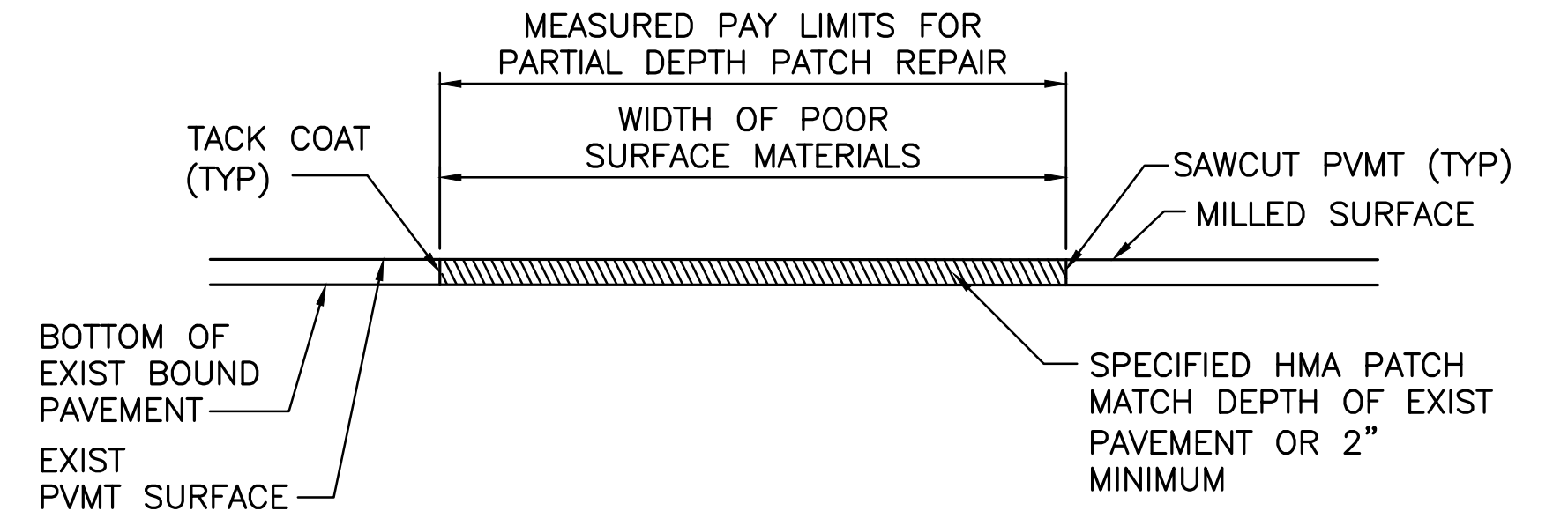
PAVEMENT OVERLAY TYPICAL DETAIL  
N.T.S.



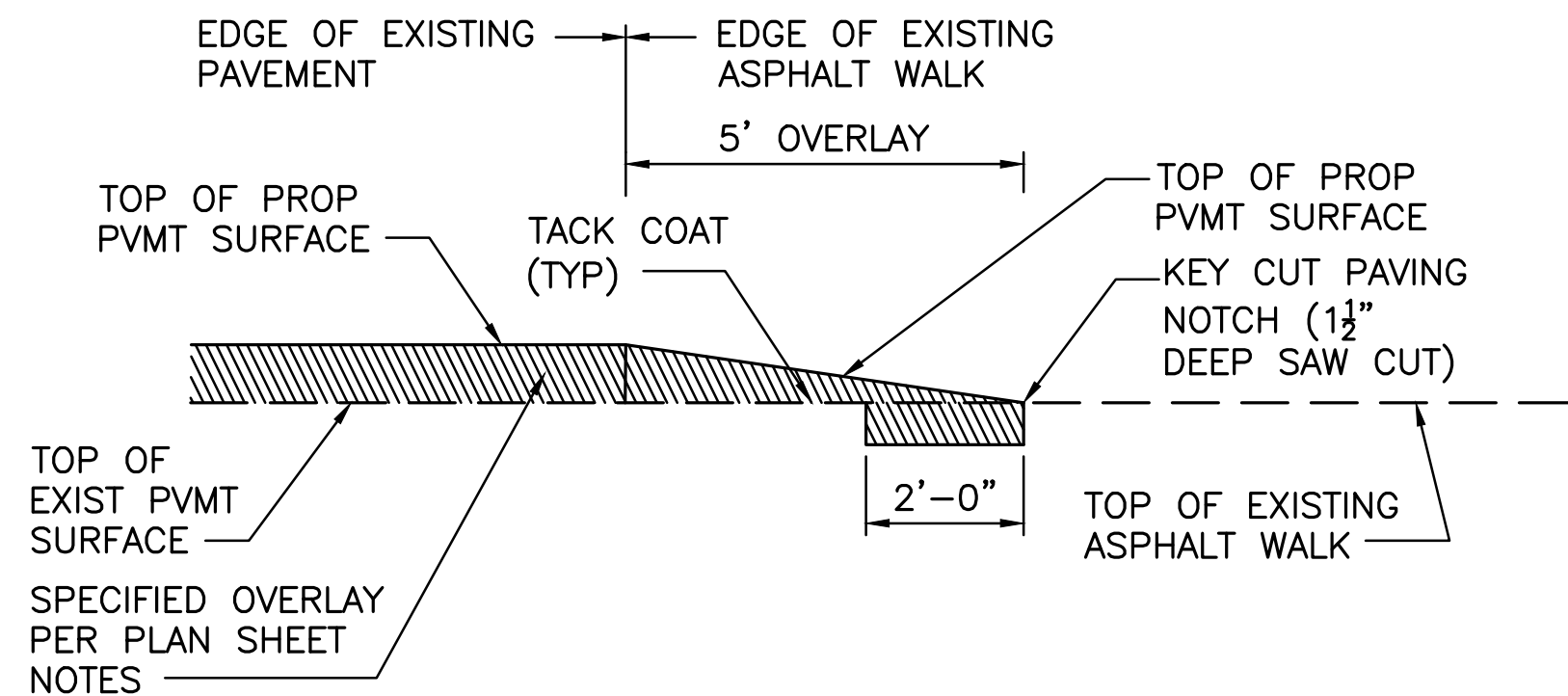
NOTES:

1. PLACEMENT OF THE AGGREGATE BASE MATERIAL SHALL BE IN TWO EQUAL LIFTS, PER SPECIFICATIONS.

FULL DEPTH PAVEMENT PATCH REPAIR DETAIL  
N.T.S.



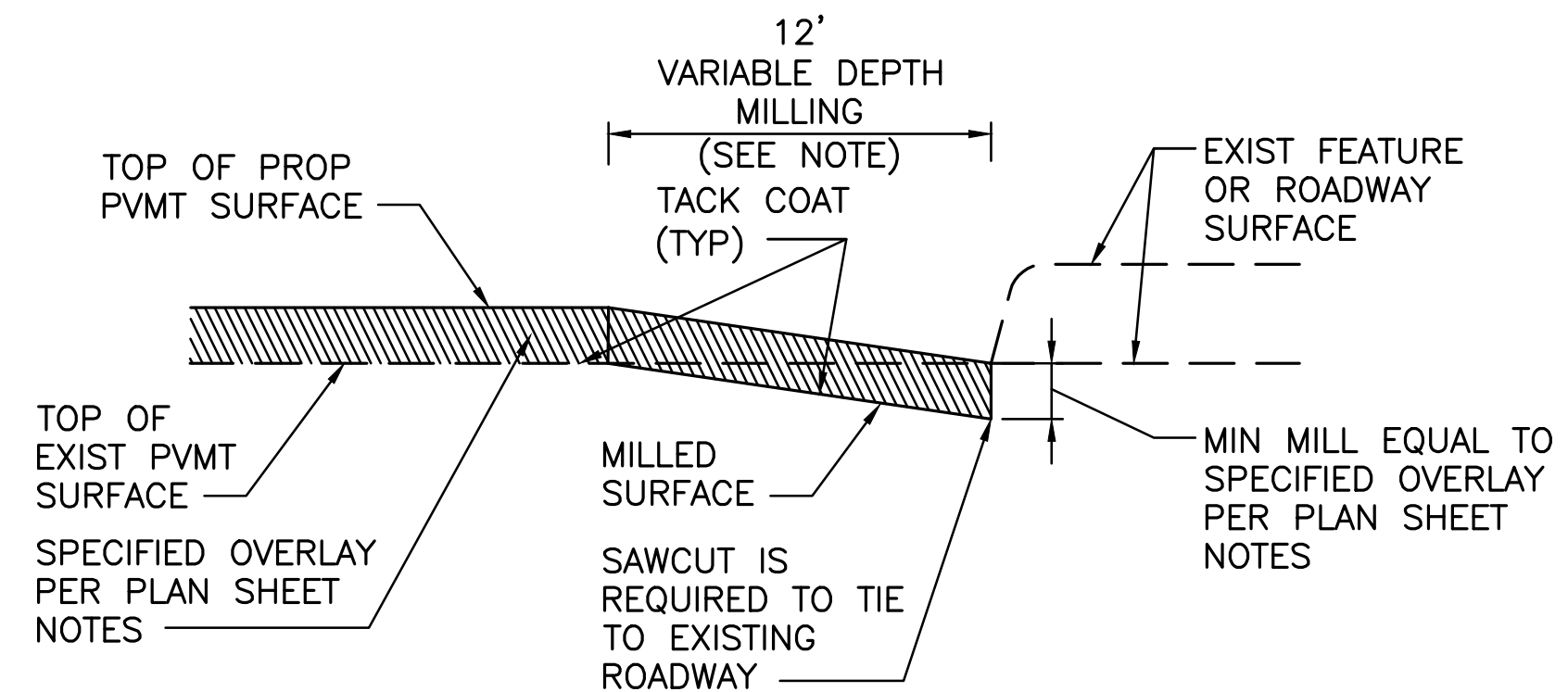
PARTIAL DEPTH PAVEMENT PATCH REPAIR DETAIL  
N.T.S.



NOTES:

1. DETAIL SHALL BE USED AT ALL LOCATIONS WHERE EXISTING ASPHALT WALKS TIE TO AREAS OF PROPOSED OVERLAY.

TRANSITION WEDGE DETAIL  
N.T.S.



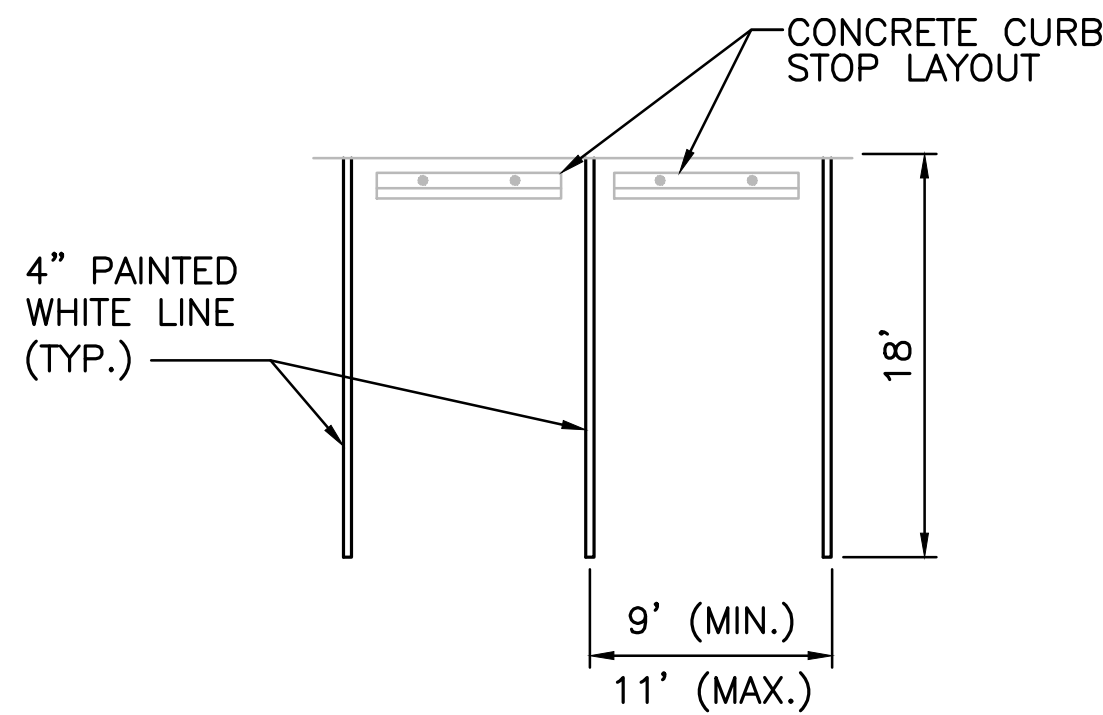
NOTES:

1. VARIABLE DEPTH MILLING WIDTH SHALL BE 50' AT INTERSECTIONS WITH EXISTING ROADWAYS FOR LEE DRIVE AND MCLAWS DRIVE.

TRANSITION MILL DETAIL  
N.T.S.

DESIGNED:	SUB SHEET NO.  C2	TITLE OF SHEET  DETAILS  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____	
CAL			PMIS/PKG NO. 147784	
CHAD				SHEET 3 OF 23
ALS				
TECH. REVIEW:				
TNR				
DATE:				
03-11-11				





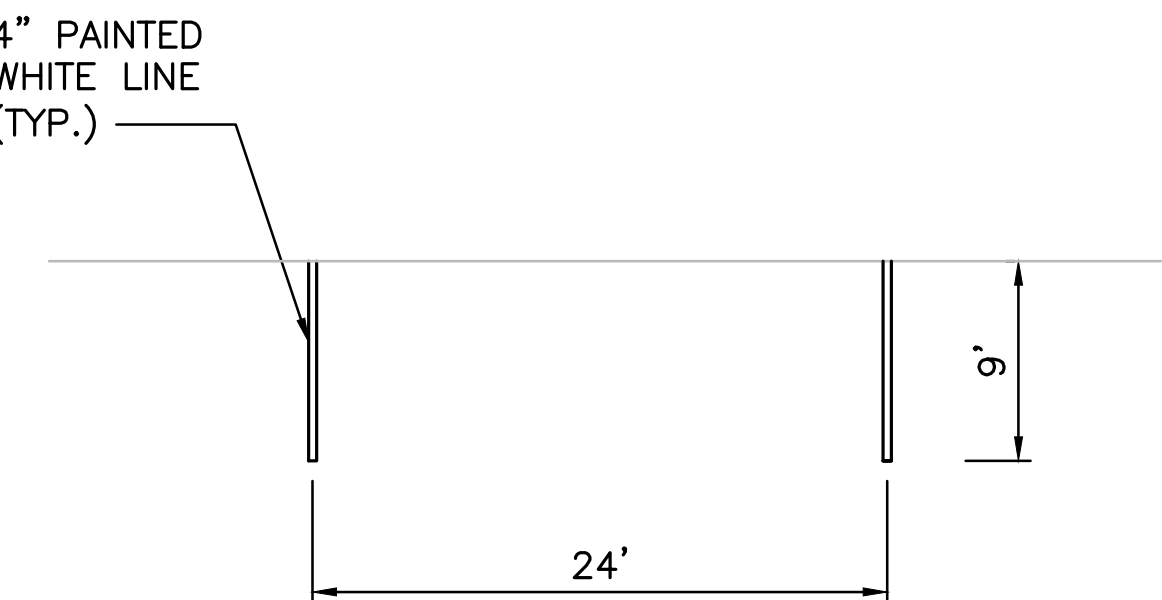
NOTES:

1. SYMBOLS & PARKING STALLS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT.
2. ALL SLOPES THROUGHOUT THE ACCESSIBLE PARKING AND AISLE AREAS SHALL NOT EXCEED 2%.
3. WIDTH OF STALLS SHALL BE FIELD VERIFIED BASED ON AVAILABLE WIDTH OF PAVEMENT IN PARKING AREA AND SHALL BE APPROVED BY C.O. PRIOR TO PAVEMENT MARKING INSTALLATION. PARKING STALL WIDTHS SHALL BE TO THE WHOLE FOOT ONLY.

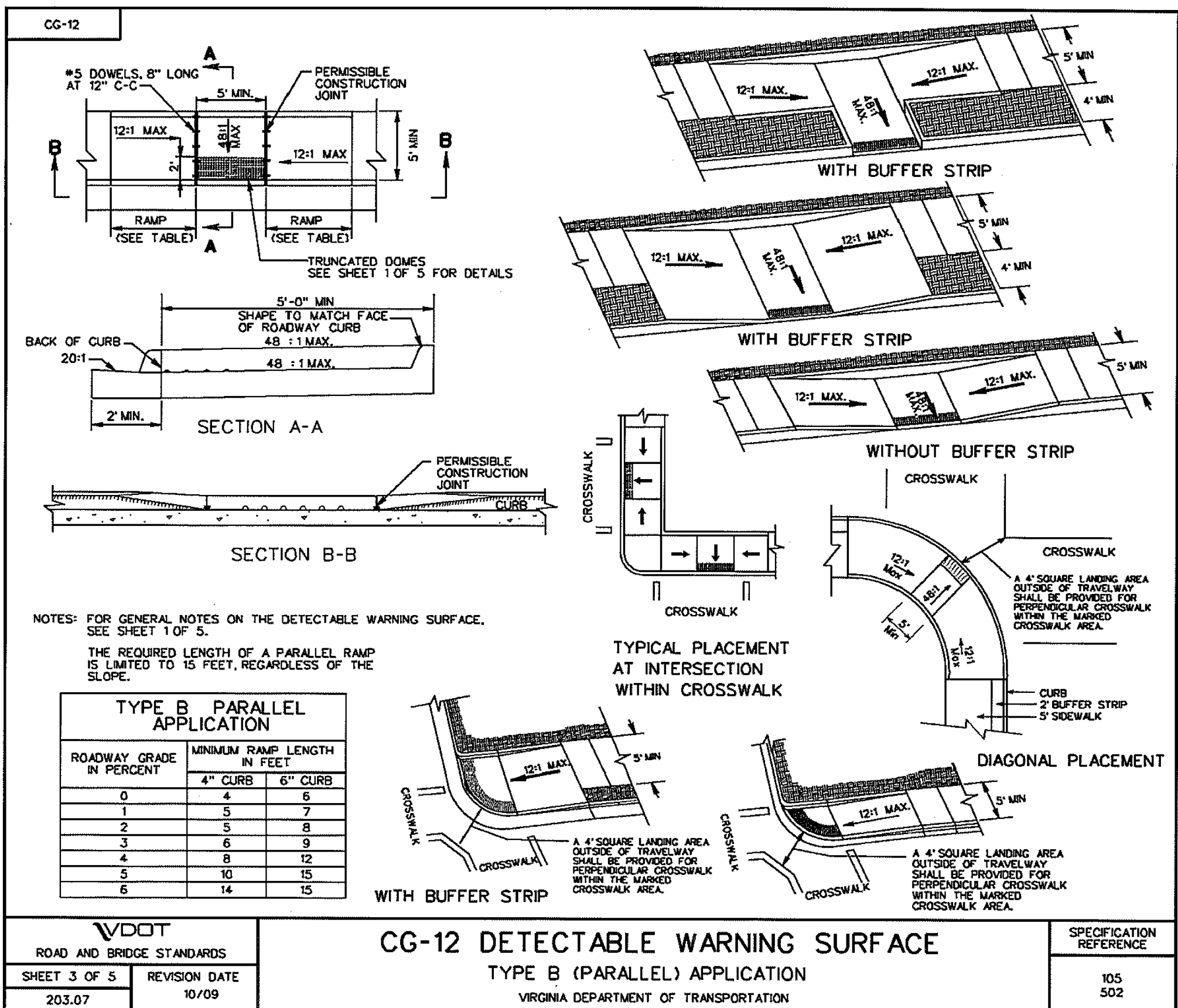
1. SYMBOLS & PARKING STALLS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT.
2. ALL SLOPES THROUGHOUT THE ACCESSIBLE PARKING AND AISLE AREAS SHALL NOT EXCEED 2%.
3. LENGTH OF STALLS SHALL BE FIELD VERIFIED BASED ON AVAILABLE WIDTH OF PAVEMENT IN PARKING AREA AND SHALL BE APPROVED BY C.O. PRIOR TO PAVEMENT MARKING INSTALLATION. PARKING STALL WIDTHS SHALL BE TO THE WHOLE FOOT ONLY.

## PAVEMENT MARKING – STALL LAYOUT


N.T.S.



N.T.S.

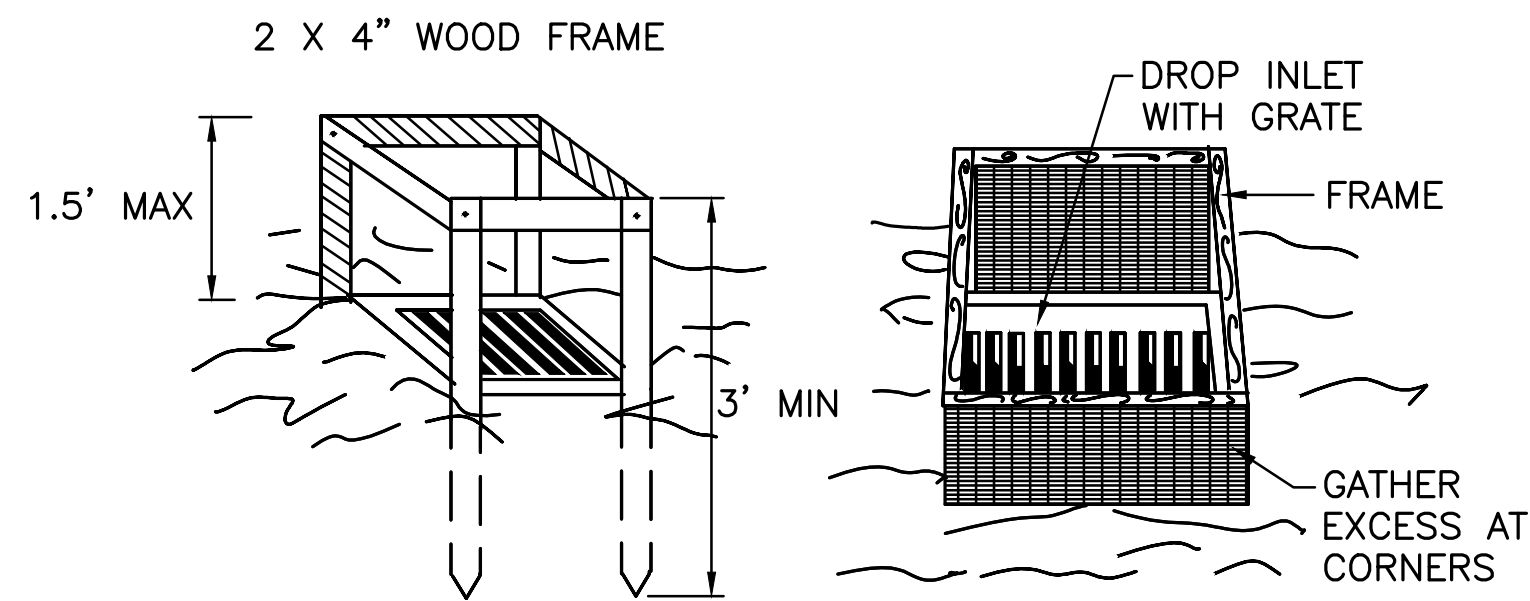


TYPE B PARALLEL APPLICATION		
ROADWAY GRADE IN PERCENT	MINIMUM RAMP LENGTH IN FEET	
	4" CURB	6" CURB
0	4	6
1	5	7
2	5	8
3	6	9
4	8	12
5	10	15
6	14	15

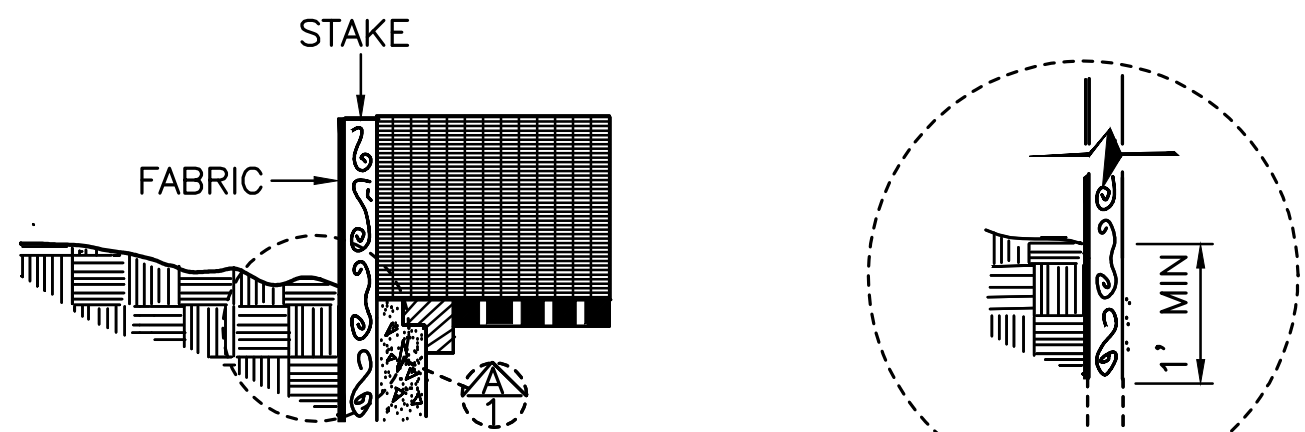
 ROAD AND BRIDGE STANDARDS		CG-12 DETECTABLE WARNING SURFACE TYPE B (PARALLEL) APPLICATION VIRGINIA DEPARTMENT OF TRANSPORTATION		SPECIFICATION REFERENCE
SHEET 3 OF 5 203.07	REVISION DATE 10/09			105 502

DESIGNED:	SUB SHEET NO.  <b>C2A</b>	TITLE OF SHEET  <b>DETAILS</b>  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____
CAL			PMIS/PKG NO. <b>147784</b>
<b>CADD</b>			
ALS			
TECH. REVIEW:			SHEET
TNR	<b>4</b> OF <b>23</b>		
DATE: 03-11-11			

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PERSPECTIVE VIEWS



ELEVATION OF STAKE AND FABRIC ORIENTATION

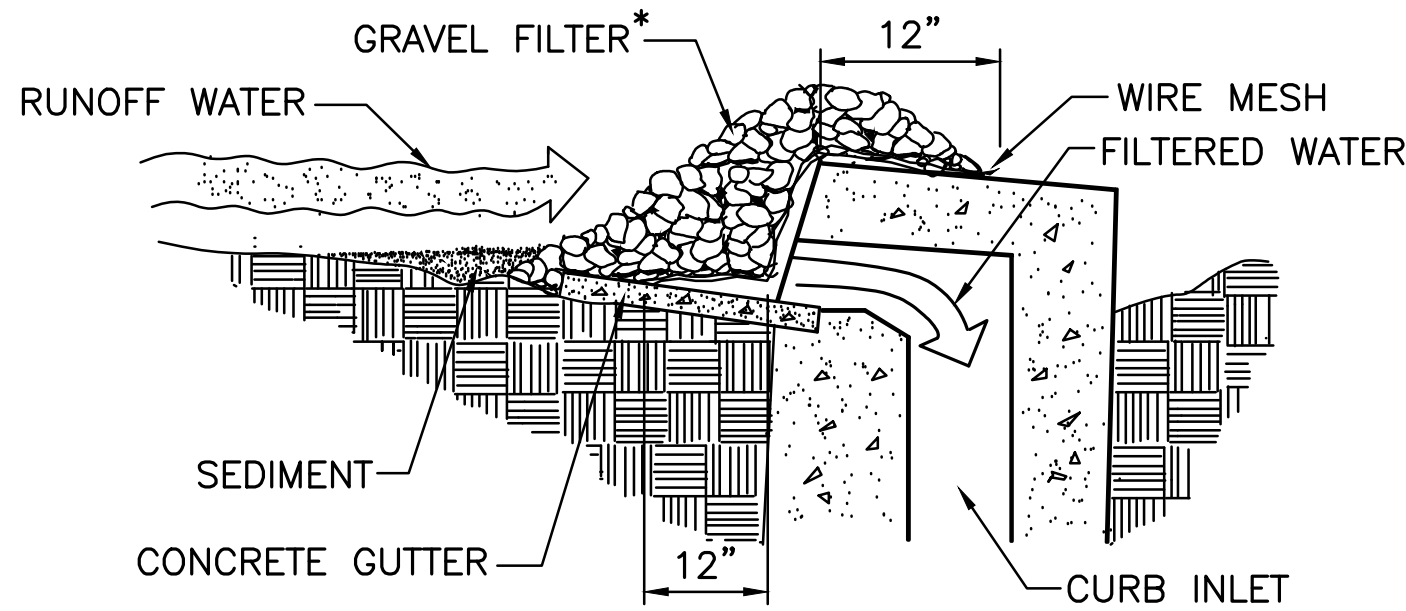
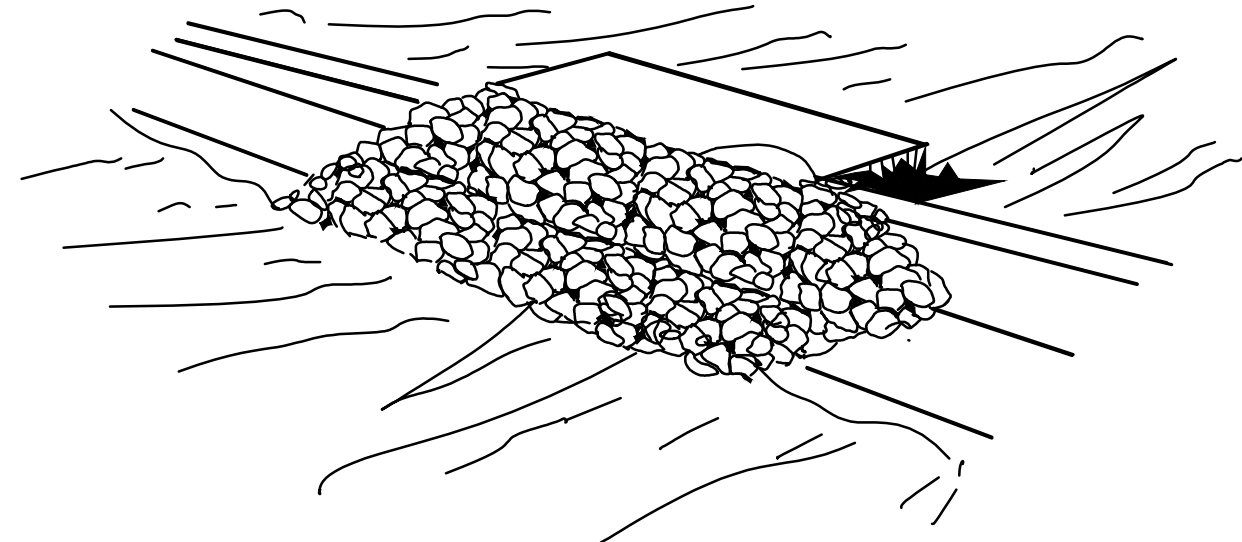
DETAIL A

SPECIFIC APPLICATION

THIS METHOD OF INLET PROTECTION IS APPLICABLE WHERE THE INLET DRAINS A RELATIVELY FLAT AREA (SLOPE NO GREATER THAN 5%) WHERE THE INLET SHEET OR OVERLAND FLOWS (NOT EXCEEDING 1 CFS) ARE TYPICAL. THE METHOD SHALL NOT APPLY TO INLETS RECEIVING CONCENTRATED FLOWS, SUCH AS IN STREET OR HIGHWAY MEDIANS.

SILT FENCE DROP INLET PROTECTION

N.T.S. Source: Virginia Erosion And Sediment Control Handbook Plate 3.07-1



SPECIFIC APPLICATION

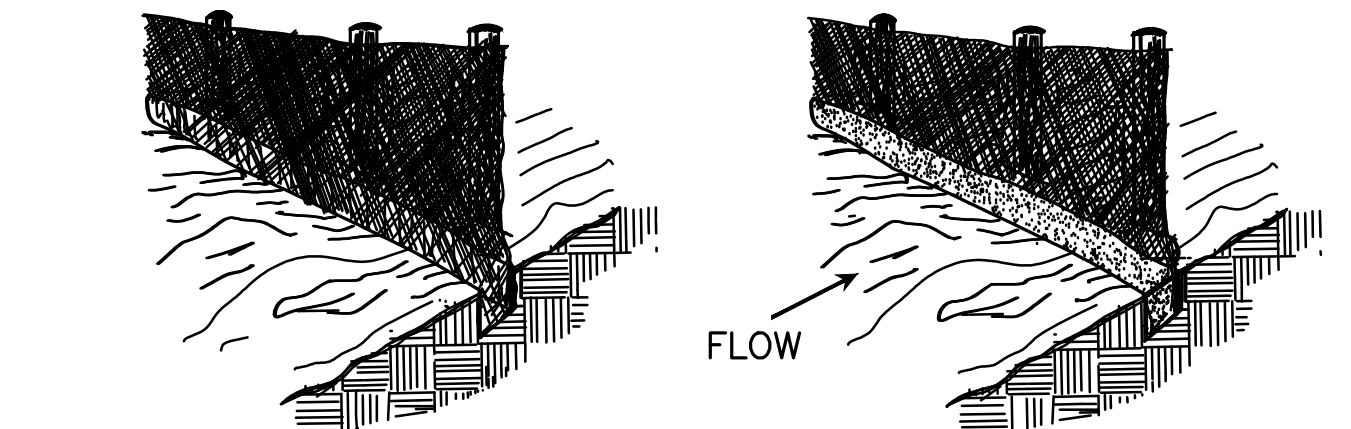
THIS METHOD OF INLET PROTECTION IS APPLICABLE AT CURB INLETS WHERE PONDING IN FRONT OF THE STRUCTURE IS NOT LIKELY TO CAUSE INCONVENIENCE OR DAMAGE TO ADJACENT STRUCTURES AND UNPROTECTED AREAS.

\* GRAVEL SHALL BE VDOT #3, #357 OR 5 COARSE AGGREGATE.

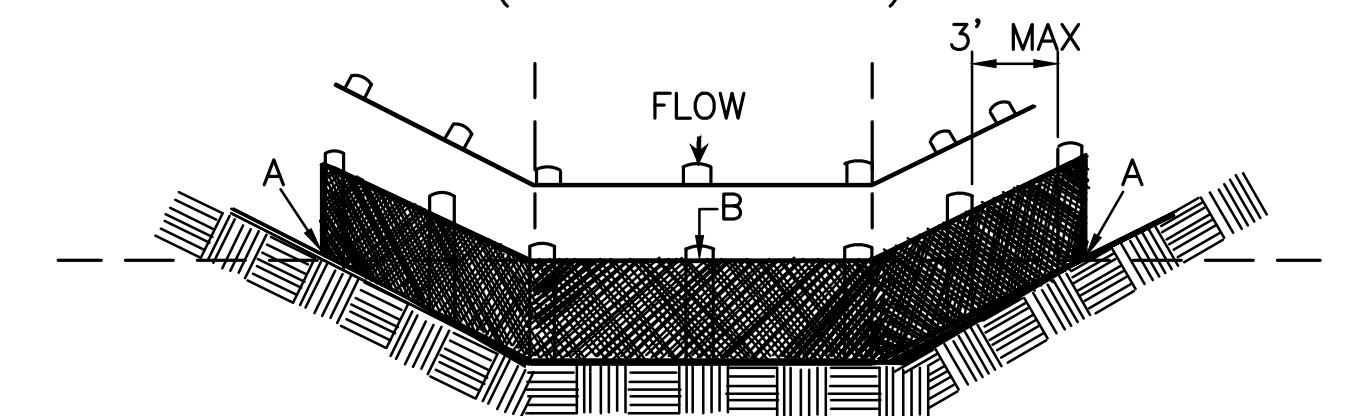
GRAVEL CURB INLET SEDIMENT FILTER

N.T.S. Source: Virginia Erosion And Sediment Control Handbook Plate 3.07-6

1. SET THE STAKES. 6' MAX
2. EXCAVATE A 4" X 4" TRENCH UPSLOPE ALONG THE LINE OF STAKES.
3. STAPLE FILTER MATERIAL TO STAKES AND EXTEND IT INTO THE TRENCH.
4. BACKFILL AND COMPACT THE EXCAVATED SOIL.



SHEET FLOW INSTALLATION (PERSPECTIVE VIEW)

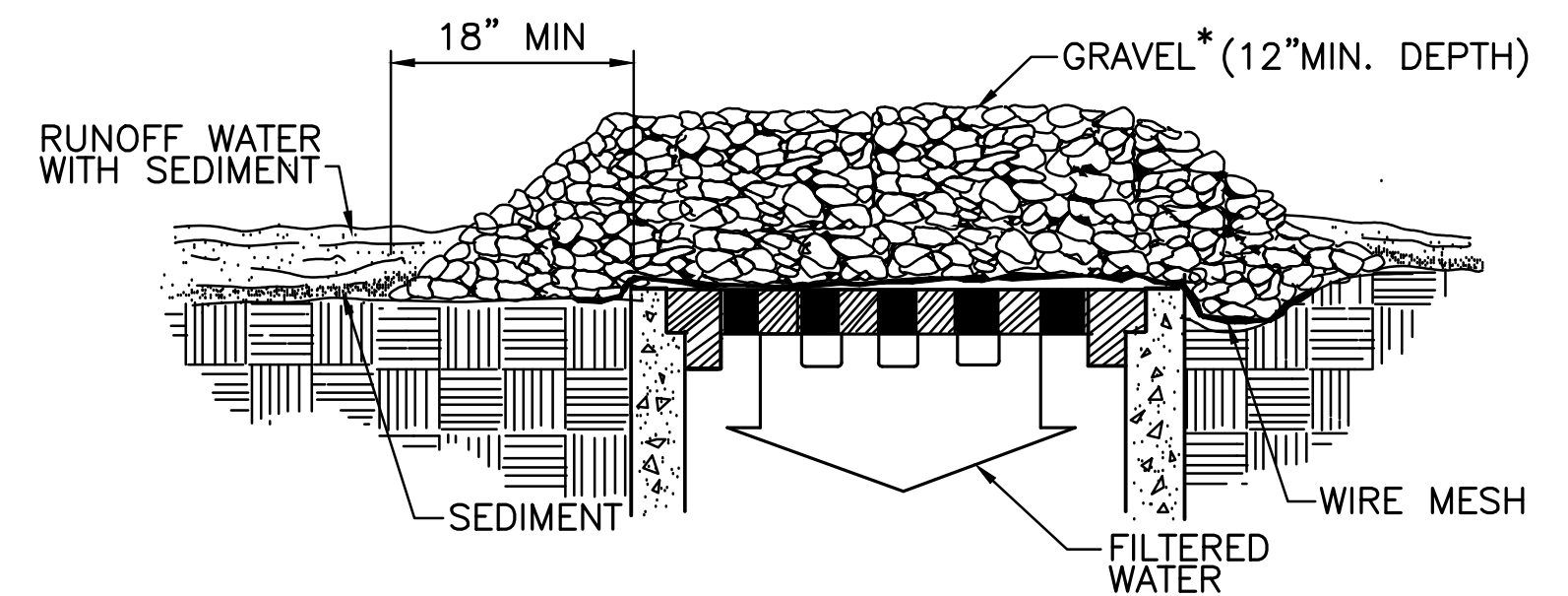


POINTS A SHOULD BE HIGHER THAN POINT B.

DRAINAGEWAY INSTALLATION (FRONT ELEVATION)

CONSTRUCTION OF A SILT FENCE (WITHOUT WIRE SUPPORT)

N.T.S. Source: Virginia Erosion And Sediment Control Handbook Plate 3.05-2



SPECIFIC APPLICATION

THIS METHOD OF INLET PROTECTION IS APPLICABLE WHERE HEAVY CONCENTRATED FLOWS ARE EXPECTED BUT NOT WHERE PONDING AROUND THE STRUCTURE MIGHT CAUSE EXCESSIVE INCONVENIENCE OR DAMAGE TO ADJACENT STRUCTURES AND UNPROTECTED AREAS.

\* GRAVEL SHALL BE VDOT #3, #357 OR #5 COARSE AGGREGATE.

GRAVEL AND WIRE MESH DROP INLET SEDIMENT FILTER

N.T.S. Source: Virginia Erosion And Sediment Control Handbook Plate 3.07-2

DESIGNED: CAL CADD ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  <b>C2B</b>	TITLE OF SHEET  <b>DETAILS</b>  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  PMIS/PKG NO. 147784 SHEET 5 OF 23
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MAINTENANCE OF TRAFFIC GENERAL NOTES

- 1) THE SUGGESTED SEQUENCE OF CONSTRUCTION IS TO BE A GUIDE FOR THE CONTRACTOR ONLY. THE CONTRACTOR, AFTER THE AWARD OF THE CONTRACT MAY SUBMIT AN ALTERNATIVE SEQUENCE OF CONSTRUCTION PLAN FOR APPROVAL BY C.O.R.

2) THE PURPOSE OF THE MAINTENANCE OF TRAFFIC PLAN IS TO ILLUSTRATE MAJOR TRAFFIC OPERATION AND IS NOT INTENDED TO SHOW EVERY DETAIL RELATED TO THE MAINTENANCE OF TRAFFIC DURING CONSTRUCTION.

3) ALL TRAFFIC MAINTENANCE AND CONTROL DEVICES SHALL CONFORM TO THE FOLLOWING PUBLICATIONS INCLUDING ALL CURRENT EDITIONS AND REVISIONS:

- VIRGINIA WORK AREA PROTECTION MANUAL

- MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, ISSUED BY THE U. S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION (MUTCD)

- APPLICABLE STANDARDS AND SPECIFICATIONS

4) AT THE END OF EACH WORK DAY THE CONTRACTOR SHALL BACKFILL ANY EXCAVATED AREAS ADJACENT TO THE TRAVEL WAY TO A SLOPE OF 6:1, UNLESS OTHERWISE INDICATED.

5) MILLED SECTIONS OF ROADWAY GREATER THAN 1.5" THAT ARE TO REMAIN OVER NIGHT SHALL HAVE A MILLED TRANSITION PERPENDICULAR AND PARALLEL TO THE ROADWAY.

6) THE CONTRACTOR SHALL MAINTAIN SAFE ACCESS FOR EMERGENCY VEHICLES WITHIN THE WORK ZONE AT ALL TIMES.

7) TEMPORARY LANE WIDTHS SHALL NOT BE LESS THAN 10 FEET.

8) GROUP II CHANNELIZING DEVICES SHALL BE SPACED 20' BETWEEN DEVICES IN TAPERS AND CURVES AND 40' BETWEEN DEVICES WHEN PARALLEL TO THE TRAVELWAY.

9) SIGN SPACING SHALL BE ADJUSTED TO A DISTANCE GREATER THAN 500' AS NECESSARY TO PROVIDE ADVANCED WARNING AROUND CURVES.
- 10) WORK HOURS:

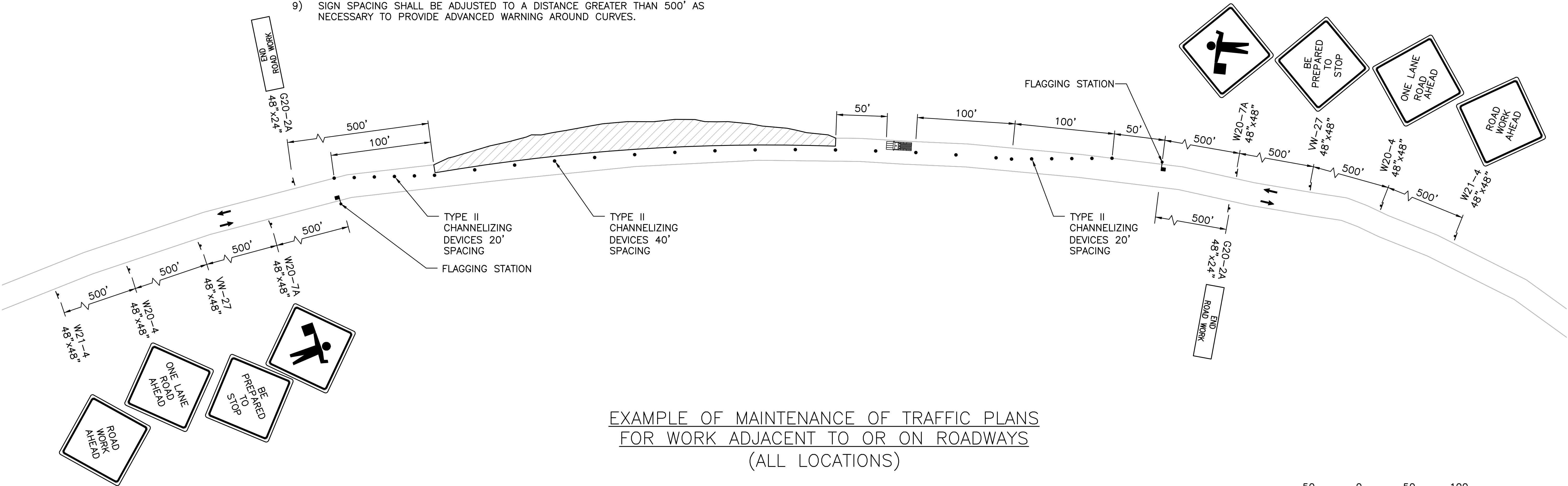
- MONDAY THROUGH FRIDAY: AT ALL OTHER LOCATIONS WORK SHALL BE COMPLETED FROM SUNUP TO SUNDOWN. NO WORK SHALL BE COMPLETED ON THE FRIDAYS PRIOR TO EASTER, MEMORIAL DAY, 4TH OF JULY AND LABOR DAY WEEKENDS.

- SATURDAYS, SUNDAYS AND HOLIDAYS: NO WORK MAY BE COMPLETED OTHERWISE NOTED OR APPROVED IN WRITING BY THE C.O. UNLESS OTHERWISE NOTED

- EXTENDED WORK HOURS AND WORK DAYS MAY BE REQUESTED WITH A WRITTEN REQUEST TO THE C.O. THIS REQUEST MUST BE SUBMITTED AT LEAST FIVE (5) WORKING DAYS PRIOR TO THE EXTENDED WORK PERIODS.

11) LEE DRIVE AND MCLAWS DRIVE SHALL REMAIN OPEN TO THE PUBLIC DURING CONSTRUCTION. TWO LANES OF TRAFFIC SHALL BE MAINTAINED AT THE END OF EACH WORK DAY.

12) THE STAGING AND STORAGE AREA SHALL BE AT MUTUALLY ACCEPTABLE LOCATION(S) TO THE CONTRACTOR AND C.O.



EXAMPLE OF MAINTENANCE OF TRAFFIC PLANS  
FOR WORK ADJACENT TO OR ON ROADWAYS  
(ALL LOCATIONS)

50 0 50 100  
SCALE OF FEET

LEGEND	
	WORK AREA
	SHADOW VEHICLE
	GROUP II CHANNELIZING DEVICES

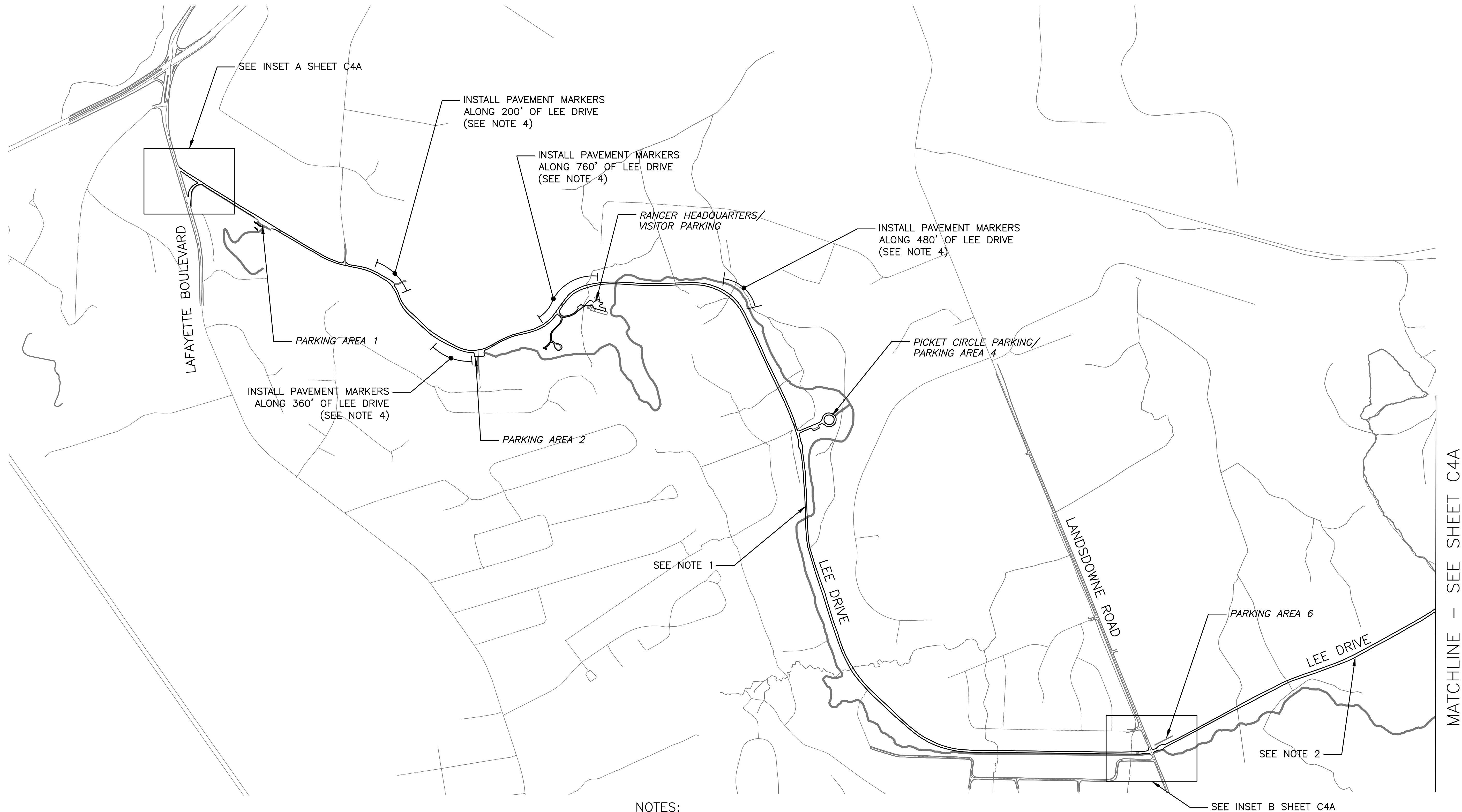
DESIGNED: CAL ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  C3	TITLE OF SHEET <b>MAINTENANCE OF TRAFFIC PLAN</b>  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  PMIS/PKG NO. 147784 SHEET 6 OF 23
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A horizontal scale bar with alternating black and white segments. It is marked with the numbers 50, 0, 50, and 100 from left to right. Below the bar, the text "SCALE OF FEET" is written in all caps.

DESIGNED:	SUB SHEET NO.	TITLE OF SHEET	DRAWING NO.
CAL	C3A	MAINTENANCE OF TRAFFIC PLAN	_____
<b>ADD</b>			
ALS			PMIS/PKG NO. 147784
TECH. REVIEW:			SHEET
TNR			7 OF 23
DATE: 03-11-11		FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	

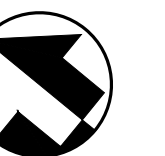
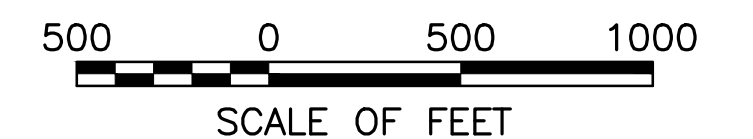


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**NOTES:**

1. 3" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF IM-19.0A IS REQUIRED ON LEE DRIVE FROM LAFAYETTE BOULEVARD TO LANDSDOWNE ROAD.
2. 2" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF IM-19.0A IS REQUIRED ON LEE DRIVE FROM LANDSDOWNE ROAD TO THE END OF LEE DRIVE.
3. PRIOR TO LEE DRIVE REPAIRS, CONTRACTOR SHALL DOCUMENT EXISTING STRIPING. UPON COMPLETION OF OVERLAY, STRIPING SHALL BE RESTORED.
4. PAVEMENT MARKERS (SNOWPLOWABLE) ON LEE DRIVE SHALL BE INSTALLED ALONG THE CENTER OF THE ROADWAY EVERY 40'. FINAL LOCATIONS SHALL BE COORDINATED WITH C.O.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
6. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.



DESIGNED: CAL	SUB SHEET NO.  <b>C4</b>	TITLE OF SHEET <b>LEE DRIVE ROUTE 10</b>  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____
CHAD			PMIS/PKG NO. 147784
ALS			SHEET
TECH. REVIEW: TNR			<b>8 of 23</b>
DATE: 03-11-11			



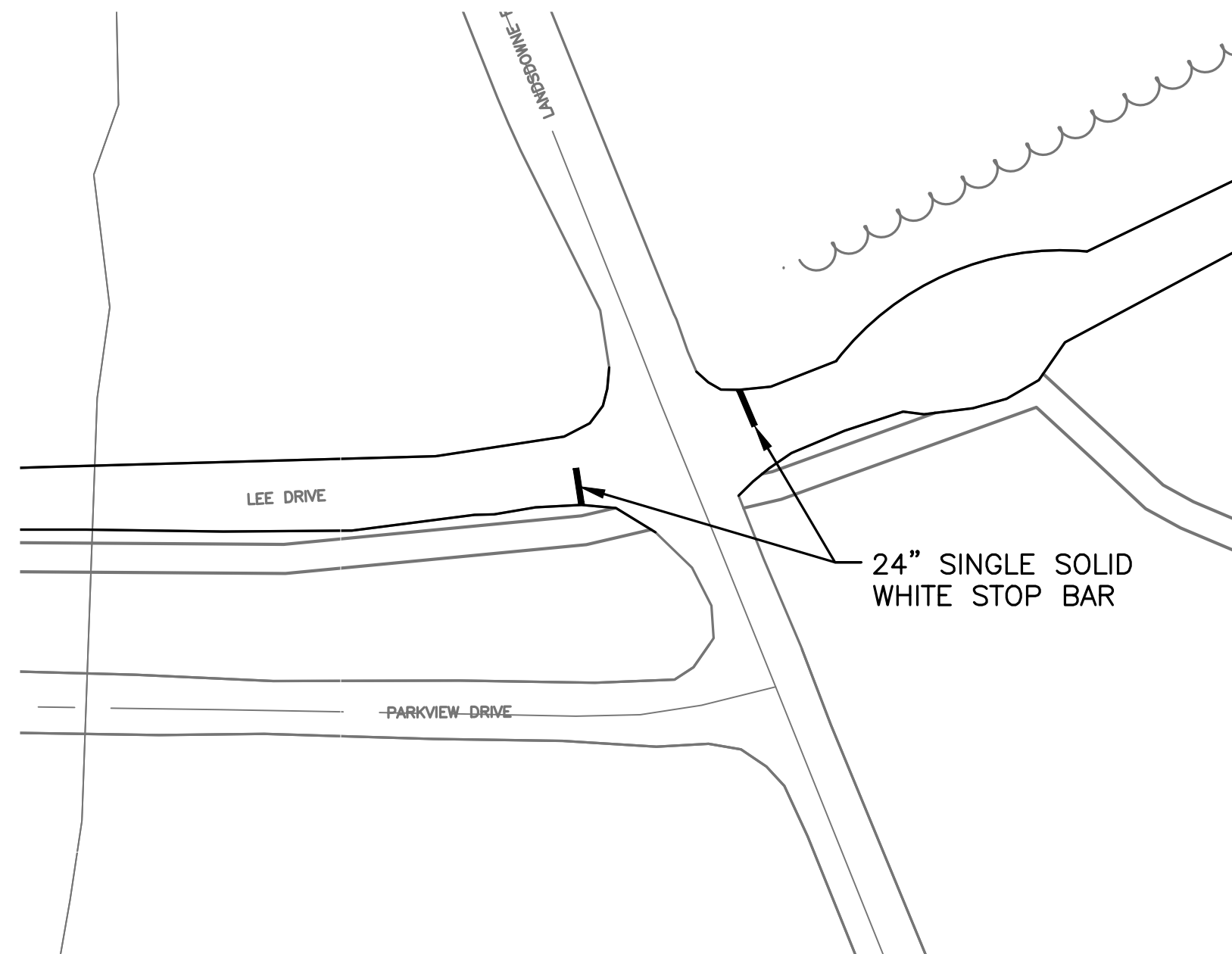
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XREFS: \\vib\proj\VaBeach\33400.03 FRSP Pavement Rehab\cad\FRSP\147784\drawings\bases\XC-MATCHLINE.dwg \\vib\proj\VaBeach\33400.03 FRSP Pavement Rehab\cad\FRSP\147784\drawings\bases\XC-SITE.dwg\33400.03 FRSP Pavement Rehab\cad\FRSP\147784\drawings\bases\DATE.dwg\XC-PROP-R....

MATCHLINE – SEE SHEET C4

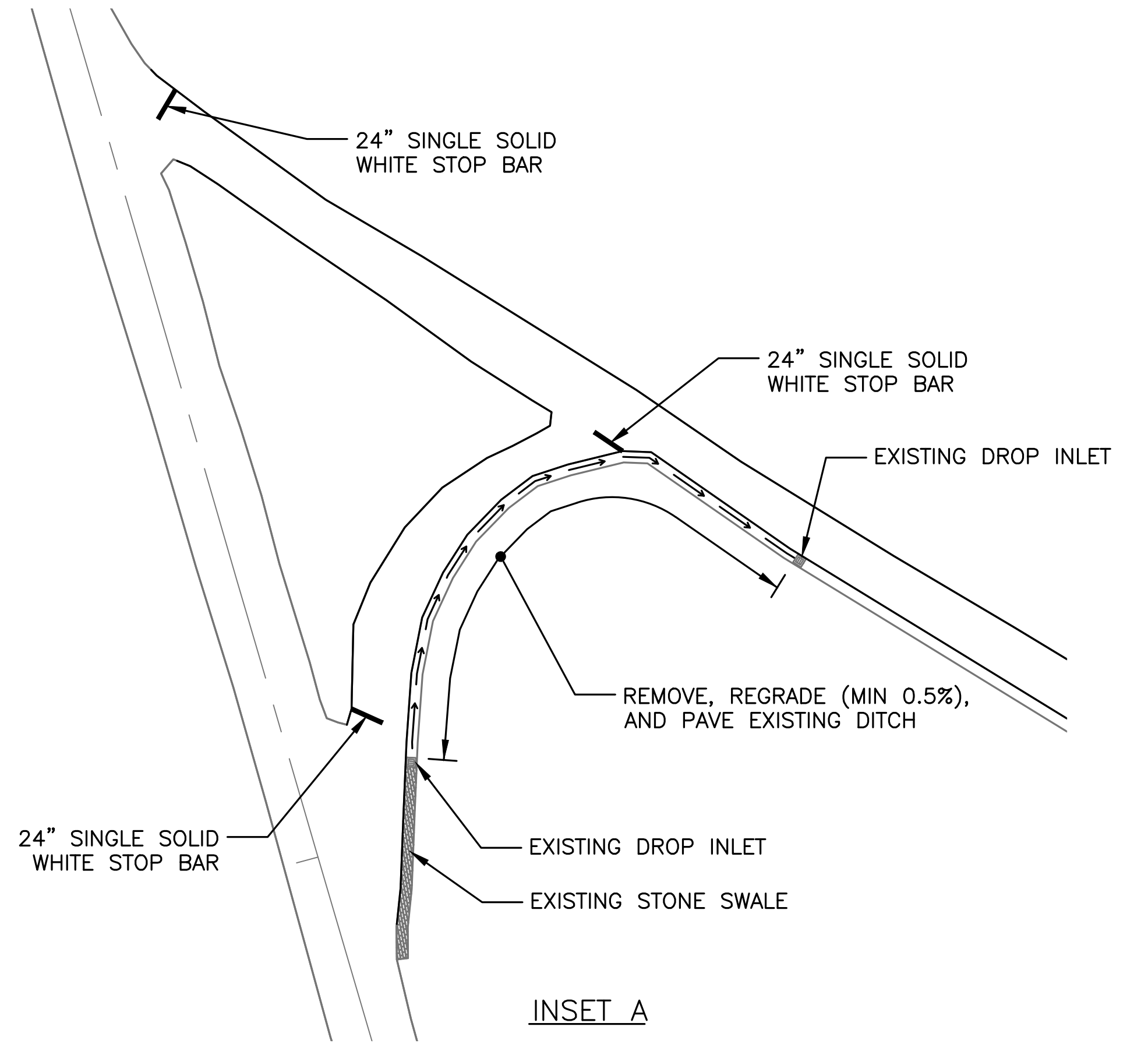


NOTES:

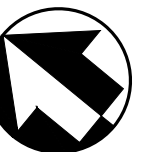
1. 3" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF IM-19.0A IS REQUIRED ON LEE DRIVE FROM LAFAYETTE BOULEVARD TO LANDSDOWNE ROAD.
2. 2" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF IM-19.0A IS REQUIRED ON LEE DRIVE FROM LANDSDOWNE ROAD TO THE END OF LEE DRIVE.
3. PRIOR TO LEE DRIVE REPAIRS, CONTRACTOR SHALL DOCUMENT EXISTING STRIPING. UPON COMPLETION OF OVERLAY, STRIPING SHALL BE RESTORED.
4. PAVEMENT MARKERS (SNOWPLOWABLE) ON LEE DRIVE SHALL BE INSTALLED ALONG THE CENTER OF THE ROADWAY EVERY 40'. FINAL LOCATIONS SHALL BE COORDINATED WITH C.O.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
6. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.



INSET B

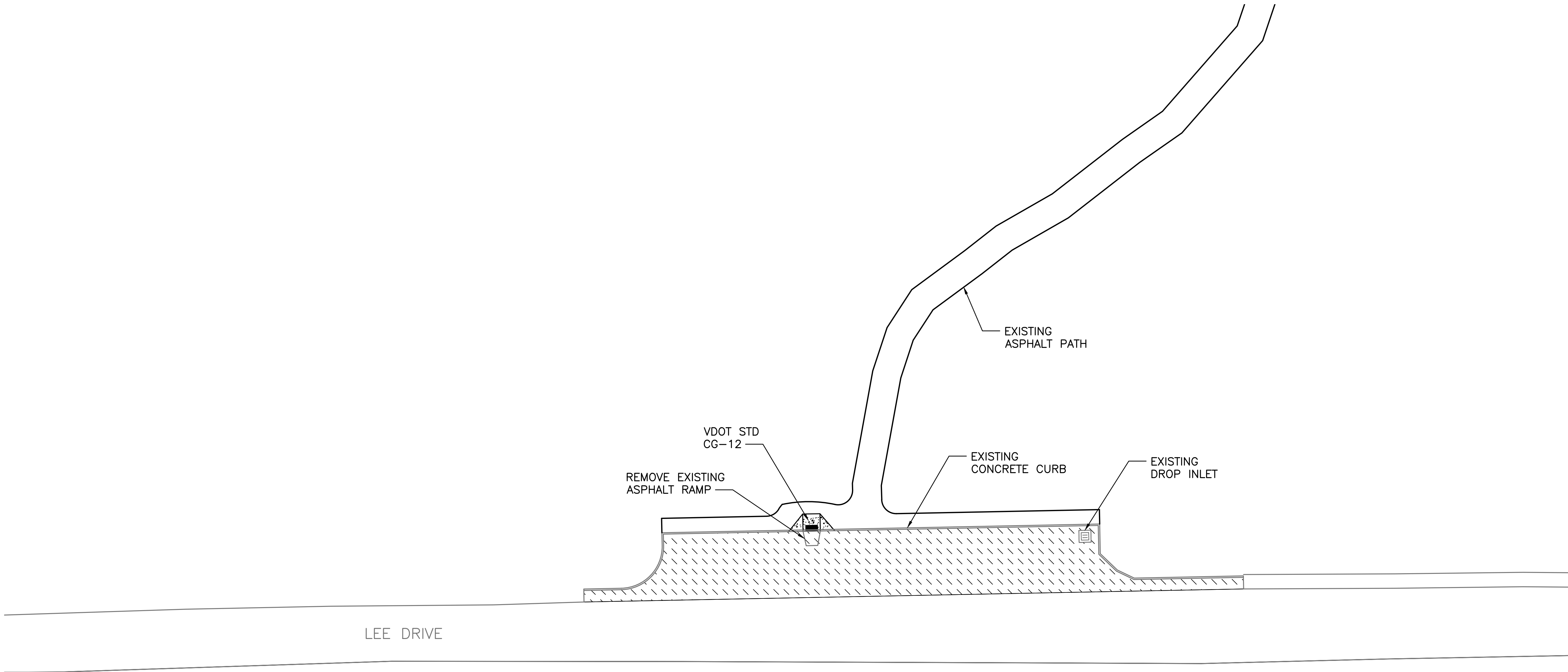


ASPHALT DITCH SECTION



DESIGNED: CAL	SUB SHEET NO.  <b>C4A</b>	TITLE OF SHEET <b>LEE DRIVE ROUTE 10</b>  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____
CHAD			PMIS/PKG NO. 147784
ALS			SHEET
TECH. REVIEW: TNR			9 of 23
DATE: 03-11-11			

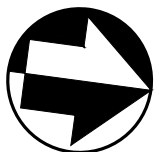
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NOTES:

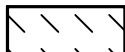


1. 3" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF SM-9.5A IN TWO EQUAL LIFTS IS REQUIRED FOR ENTIRE PAVEMENT AREA.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
3. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.

LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT
	CONCRETE



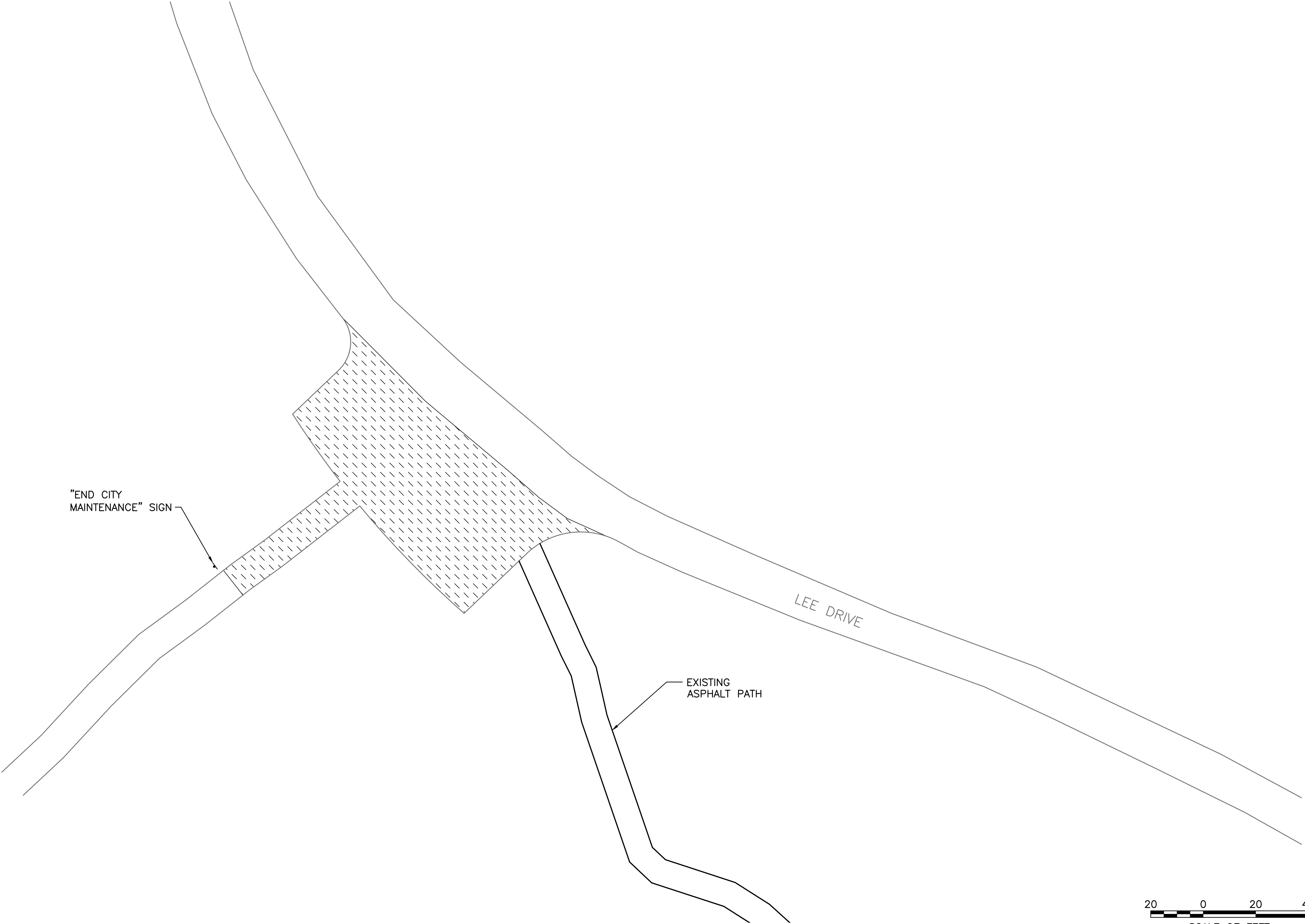
DESIGNED: CAL GADD ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  C5	TITLE OF SHEET  PARKING AREA 1 ROUTE 905  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____  PMIS/PKG NO. 147784  SHEET 10 OF 23
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3/10/11 08:13 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\C6-PARKINGAREA2.DWG  
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LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT
	CONCRETE

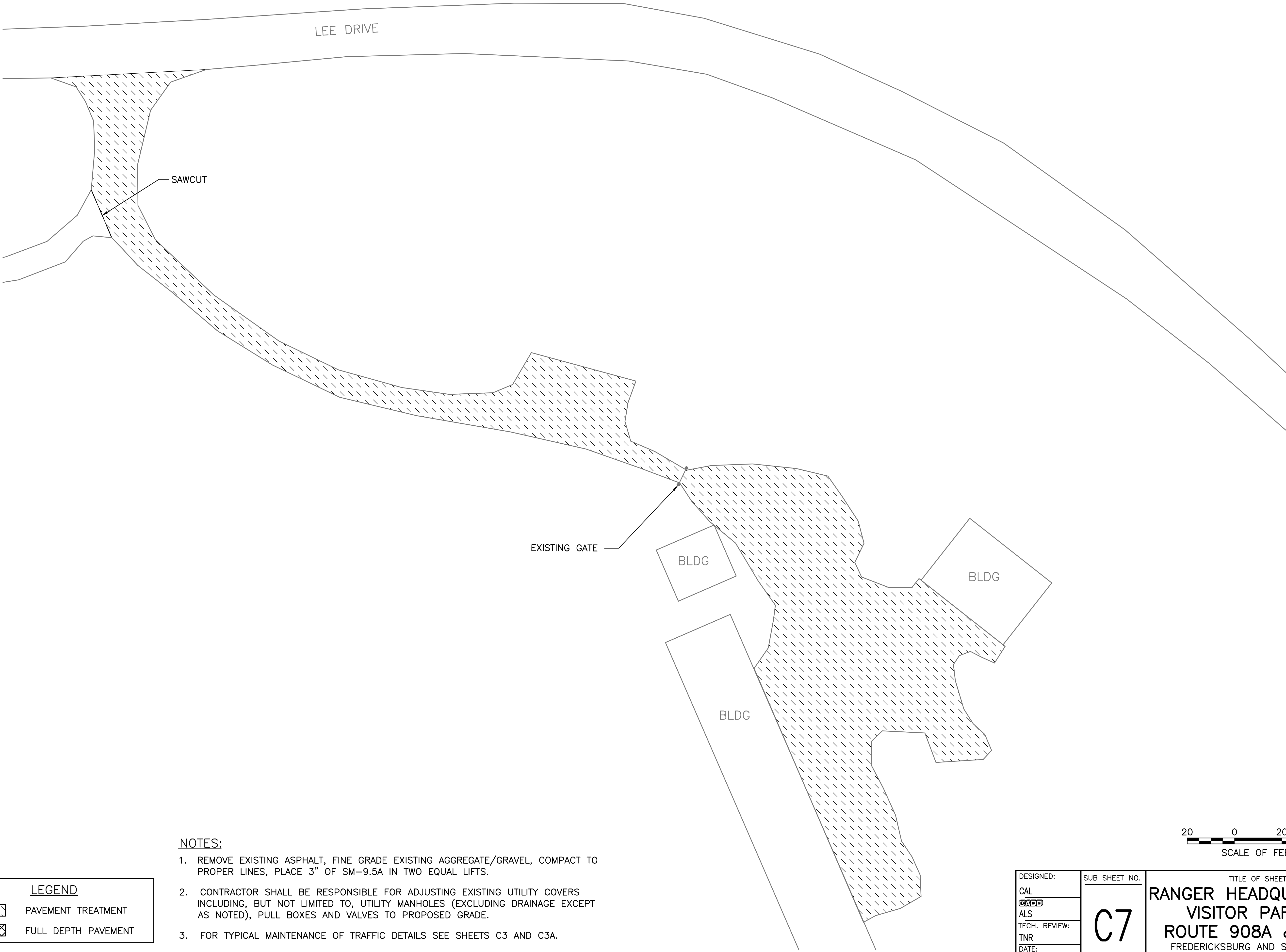
NOTES:

1. 3" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF SM-9.5A IN TWO EQUAL LIFTS IS REQUIRED FOR ENTIRE PAVEMENT AREA.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
3. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.



DESIGNED: CAL  ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  C6	TITLE OF SHEET <b>PARKING AREA 2 ROUTE 906</b>  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO. _____  PMIS/PKG NO. 147784  SHEET 11 OF 23
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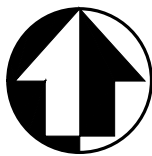
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NOTES:

1. REMOVE EXISTING ASPHALT, FINE GRADE EXISTING AGGREGATE/GRAVEL, COMPACT TO PROPER LINES, PLACE 3" OF SM-9.5A IN TWO EQUAL LIFTS.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
3. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.

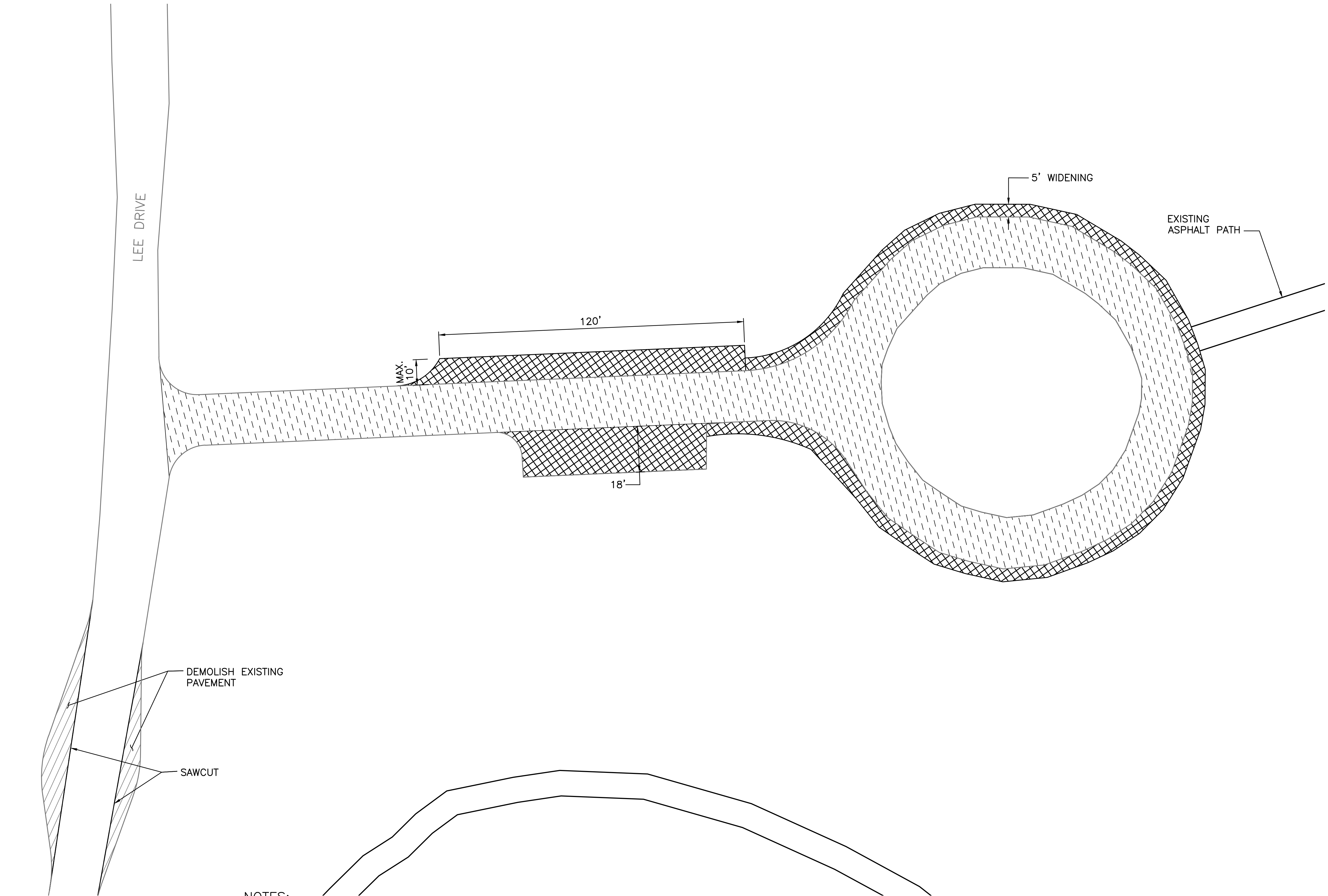
LEGEND	
	PAVEMENT TREATMENT
	FULL DEPTH PAVEMENT



DESIGNED: CAL GADD ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  C7	TITLE OF SHEET <b>RANGER HEADQUARTERS/ VISITOR PARKING</b> ROUTE 908A & 908B FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO. _____ PMIS/PKG NO. 147784 SHEET 12 of 23
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3/11/11 11:52 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\C8-PARKINGAREA4.DWG  
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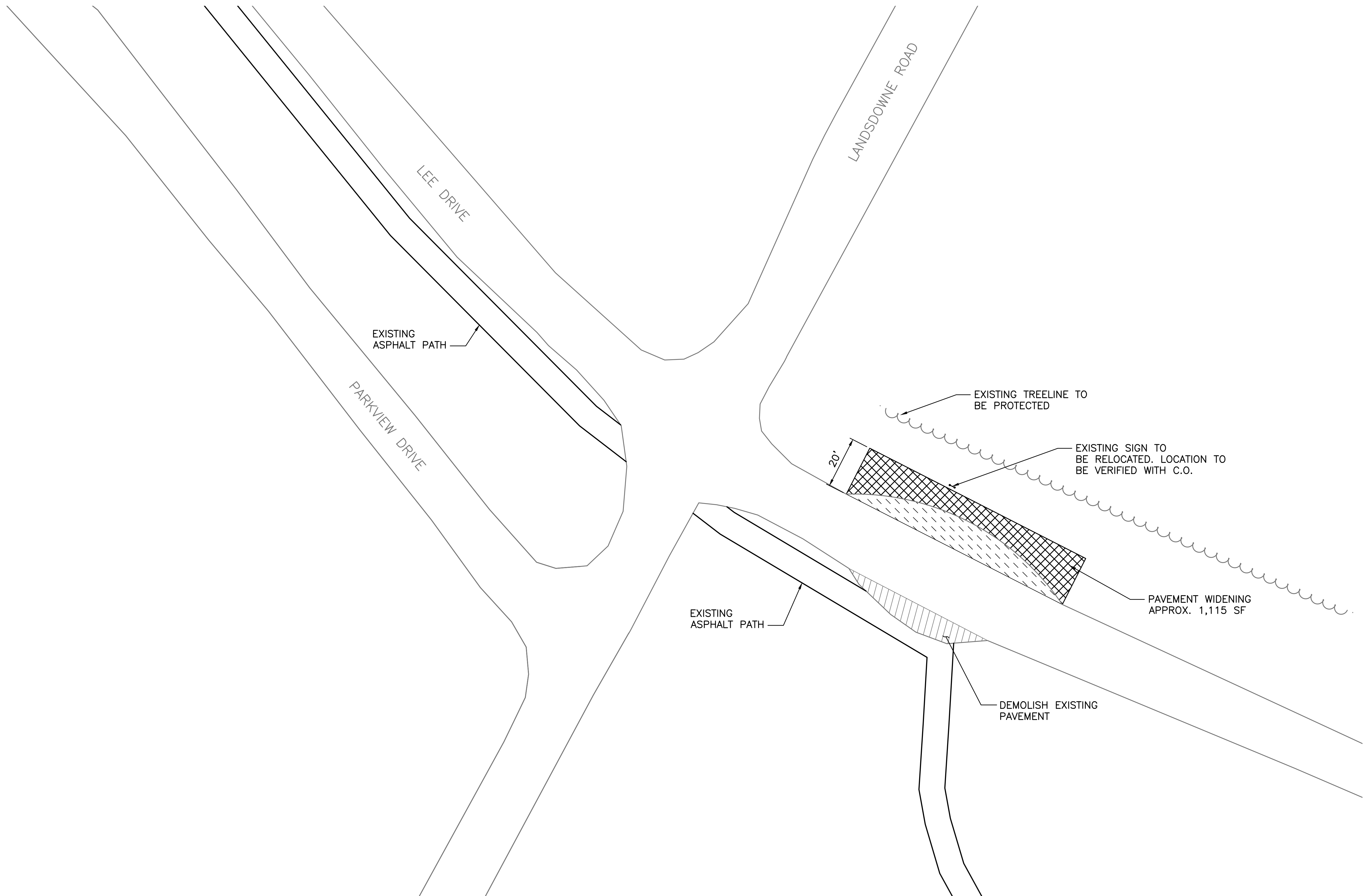


NOTES:

1. 3" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF SM-9.5A IN TWO EQUAL LIFTS IS REQUIRED FOR OVERLAY AREA.
2. FULL DEPTH PAVEMENT SECTION REQUIRES 12' DEEP EXCAVATION AND REMOVAL OF EXISTING SURFACE PRIOR TO PLACING 3" SM-9.5A IN TWO EQUAL LIFTS ON 9" AGGREGATE 21A.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
4. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.

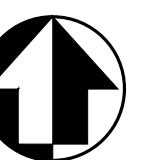
LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT
	EXISTING PAVEMENT

DESIGNED: CAL ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  C8	TITLE OF SHEET  PARKING AREA 4 AND PICKETT CIRCLE PARKING ROUTE 931 & 910A FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  PMIS/PKG NO. 147784 SHEET 13 OF 23
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NOTES:

1. 2" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF SM-9.5A IN TWO EQUAL LIFTS IS REQUIRED FOR ENTIRE PAVEMENT AREA.
2. FULL DEPTH PAVEMENT SECTION REQUIRES 12" DEEP EXCAVATION AND REMOVAL OF EXISTING SURFACE PRIOR TO PLACING WOVEN FABRIC ON THE SUBGRADE WITH 3" SM-9.5A IN TWO EQUAL LIFTS ON 9" AGGREGATE 21A.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
4. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.

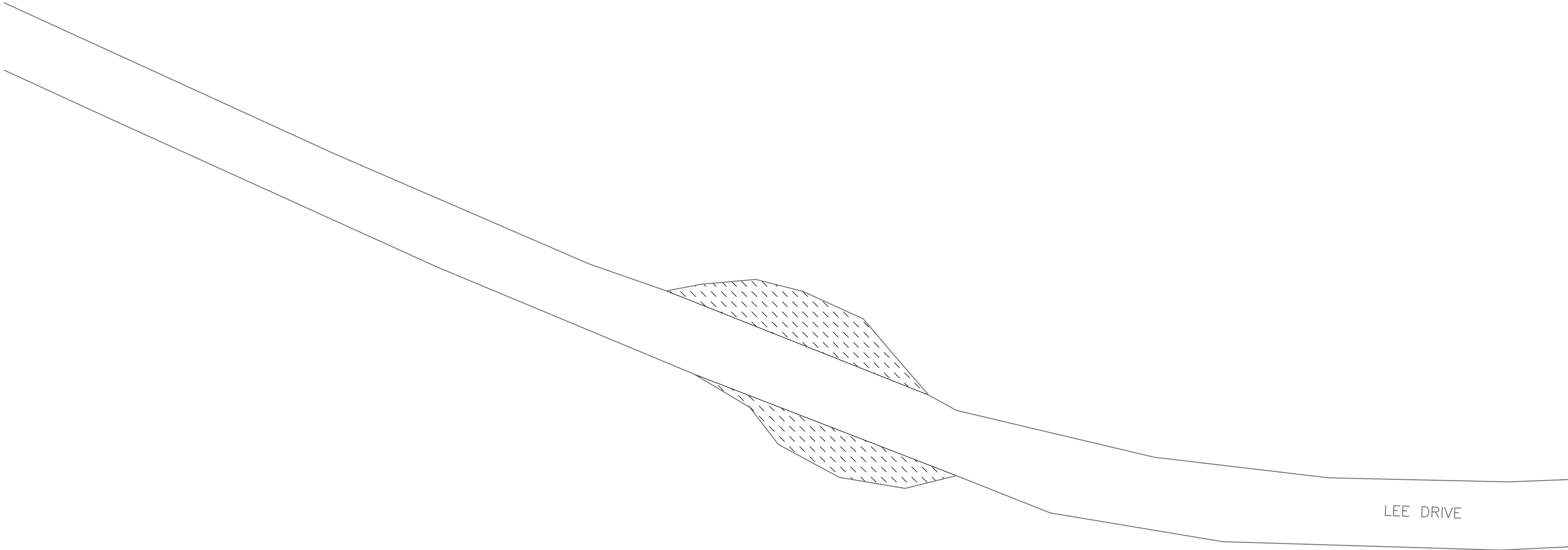


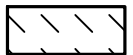

LEGEND

	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT
	EXISTING PAVEMENT

DESIGNED:	SUB SHEET NO.	TITLE OF SHEET	DRAWING NO.
CAL	C9	LEE DRIVE PARKING AREA 6 ROUTE 933 FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	_____
<b>CADD</b>			PMIS/PKG NO. 147784
ALS			SHEET
TECH. REVIEW:			14 OF 23
TNR			
DATE:			
03-11-11			

3/10/11 08:15 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\C10-PARKINGAREA5.DWG  
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LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT

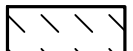

NOTES:

- 2" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF SM-9.5A IN TWO EQUAL LIFTS IS REQUIRED FOR ENTIRE PAVEMENT AREA.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
- FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.



DESIGNED: CAL	SUB SHEET NO.  C10	TITLE OF SHEET  PARKING AREA 5A & AREA 5B ROUTE 932A & 932B FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____
TECH. REVIEW: TNR			PMIS/PKG NO. 147784
DATE: 03-11-11			SHEET 15 OF 23
			

3/10/11 08:15 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\C11-UNIONBREAKTHRU.DWG  
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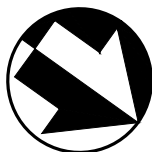
LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT

NOTES:

- 2" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF SM-9.5A IN TWO EQUAL LIFTS IS REQUIRED FOR ENTIRE PAVEMENT AREA.
- FULL DEPTH PAVEMENT SECTION REQUIRES 12" DEEP EXCAVATION AND REMOVAL OF EXISTING SURFACE PRIOR TO PLACING WOVEN FABRIC ON THE SUBGRADE WITH 3" SM-9.5A IN TWO EQUAL LIFTS ON 9" AGGREGATE 21A.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
- FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.

PAVEMENT WIDENING  
APPROX. 375 SF

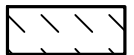

3' WIDENING



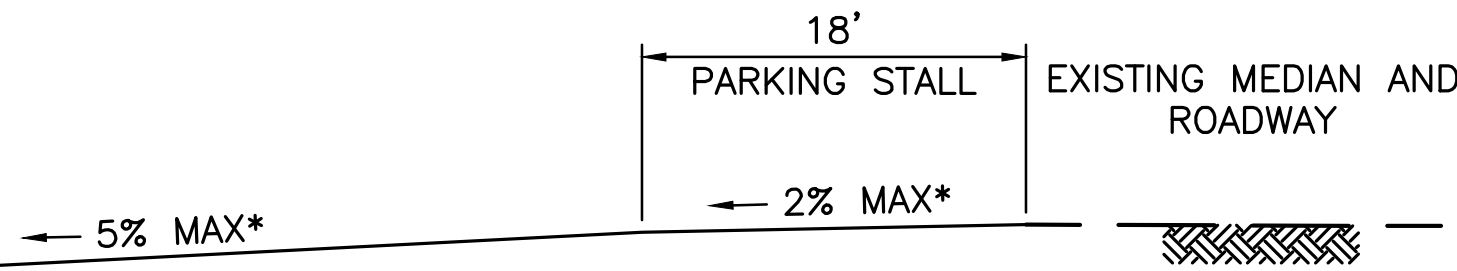
DESIGNED: CAL GADD ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  C11	TITLE OF SHEET  UNION BREAKTHROUGH  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO. _____  PMIS/PKG NO. 147784  SHEET 16 OF 23
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3/10/11 08:20 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\12-PARKINGAREA3.DWG  
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LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT

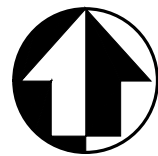
SECTION A-A  
PARKING LOT GRADING  
\* CONTRACTOR TO FIELD VERIFY GRADES PRIOR TO CONSTRUCTION.



EXISTING  
WOODEN STAIRS

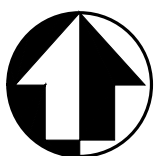
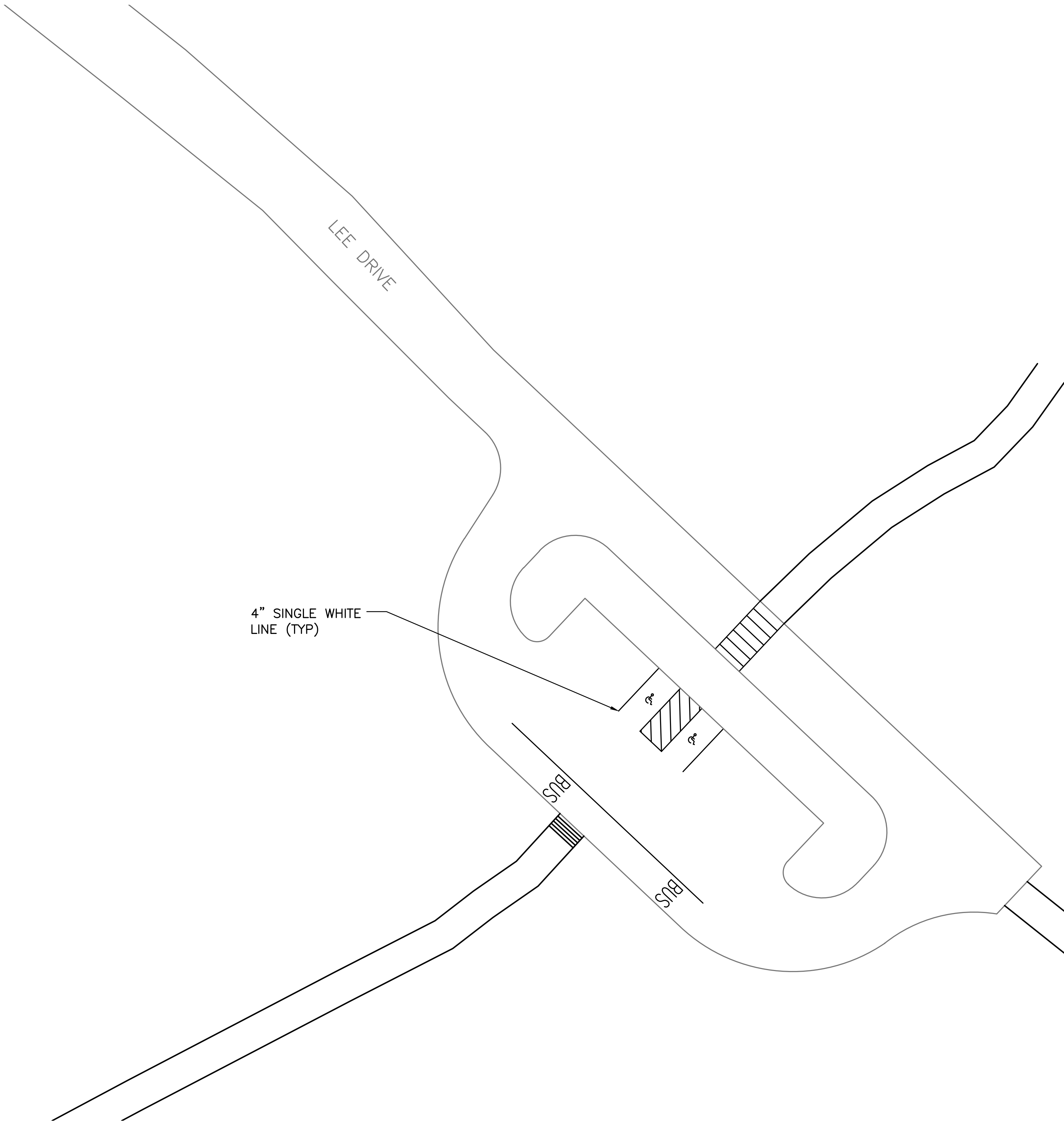
NOTES:

- 2" MILL, REPAIR LOCAL BASE FAILURES, TACK COAT AND 3" OVERLAY OF SM-9.5A IN TWO EQUAL LIFTS IS REQUIRED FOR ENTIRE PAVEMENT AREA.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
- FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.



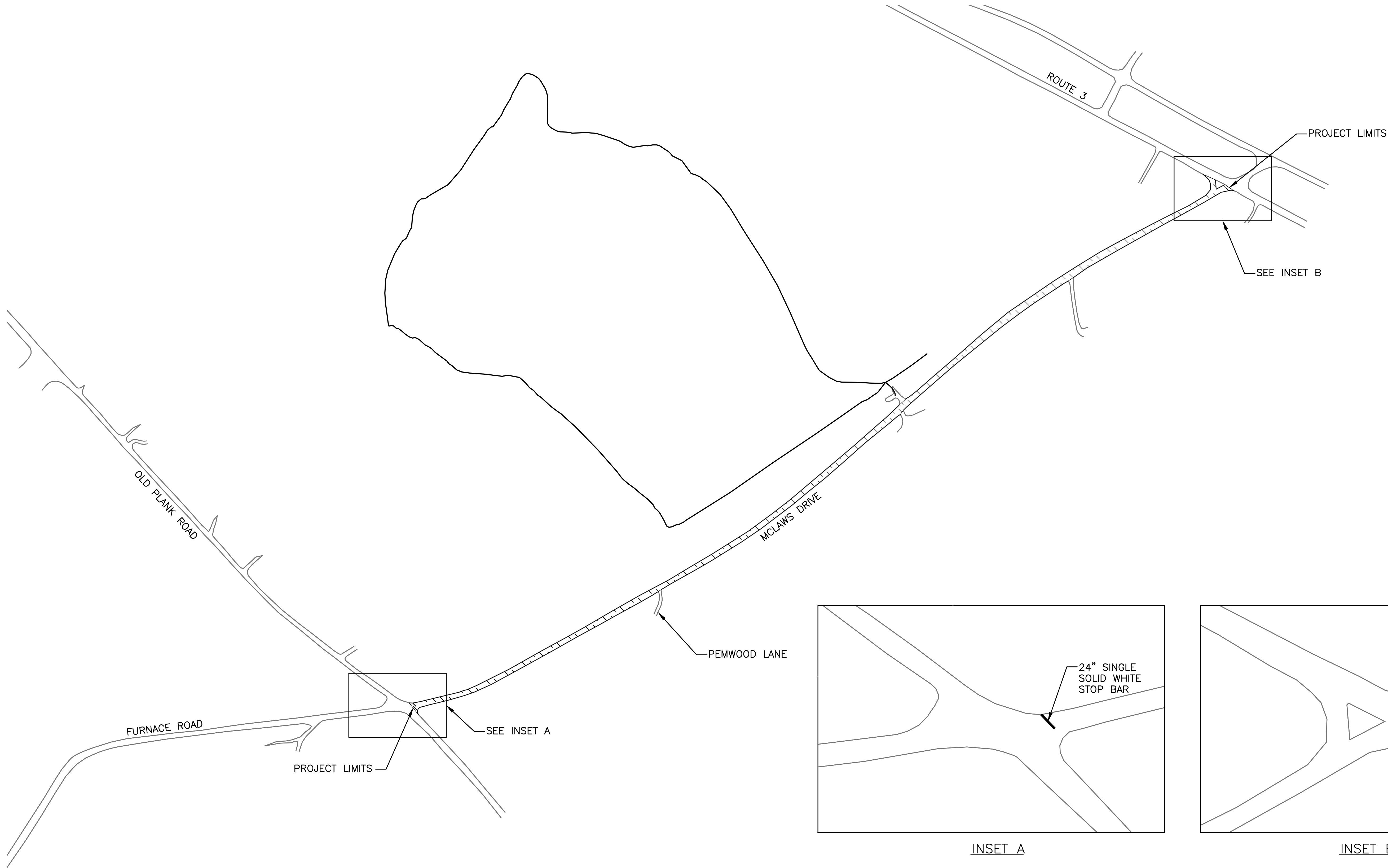
DESIGNED: CAL GADD ALS TECH. REVIEW: TNR DATE: 03-11-11	SUB SHEET NO.  C12	TITLE OF SHEET LEE DRIVE PARKING AREA 3 ROUTE 907 FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO. _____ PMIS/PKG NO. 147784 SHEET 17 OF 23
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3/10/11 08:23 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\C12A-PARKINGAREA3-PVMTMRK.DWG  
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DESIGNED: CAL	SUB SHEET NO.  <b>C12A</b>	TITLE OF SHEET <b>LEE DRIVE PARKING AREA 3 ROUTE 907 PAVEMENT MARKING PLAN</b> FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO. _____
TECH. REVIEW: TNR			PMIS/PKG NO. 147784
DATE: 03-11-11			SHEET <b>18 of 23</b>

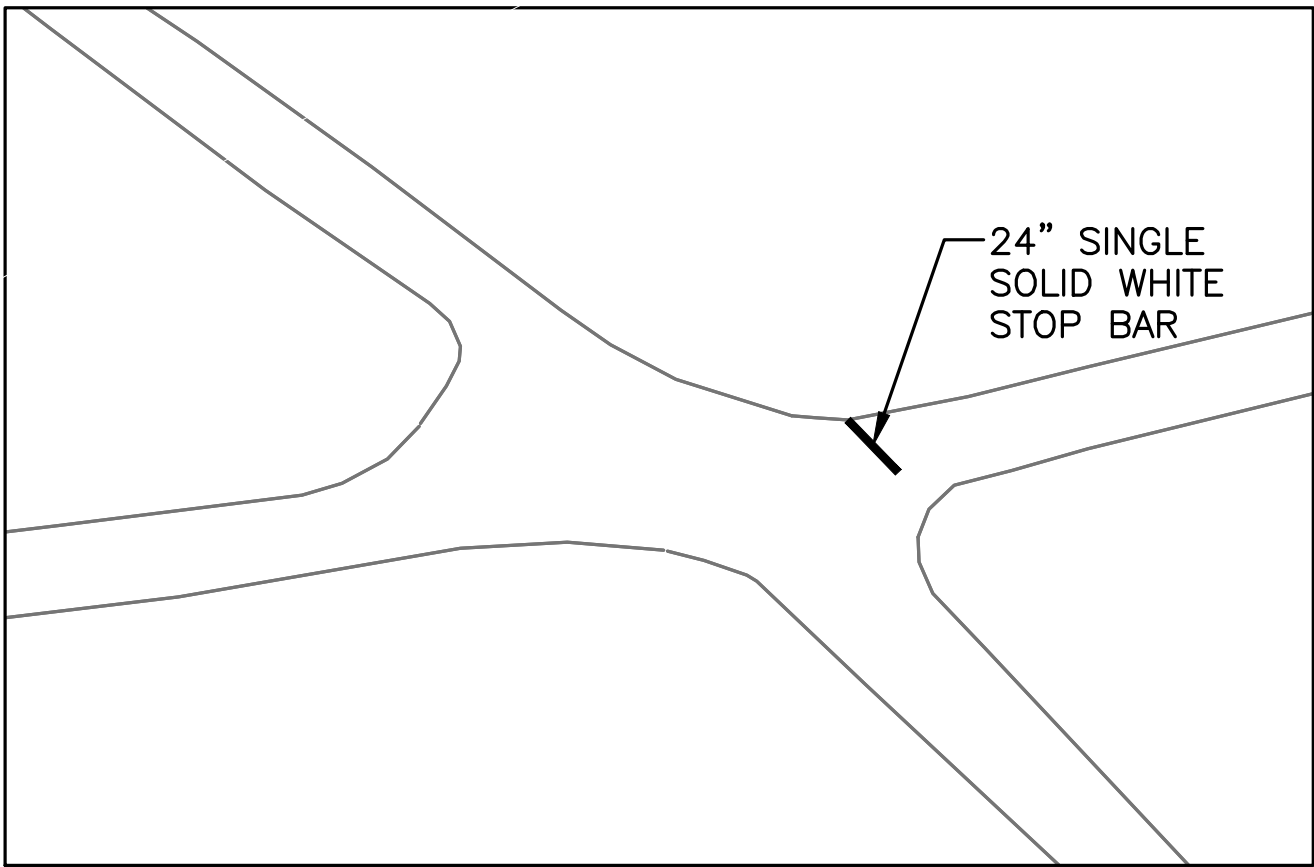
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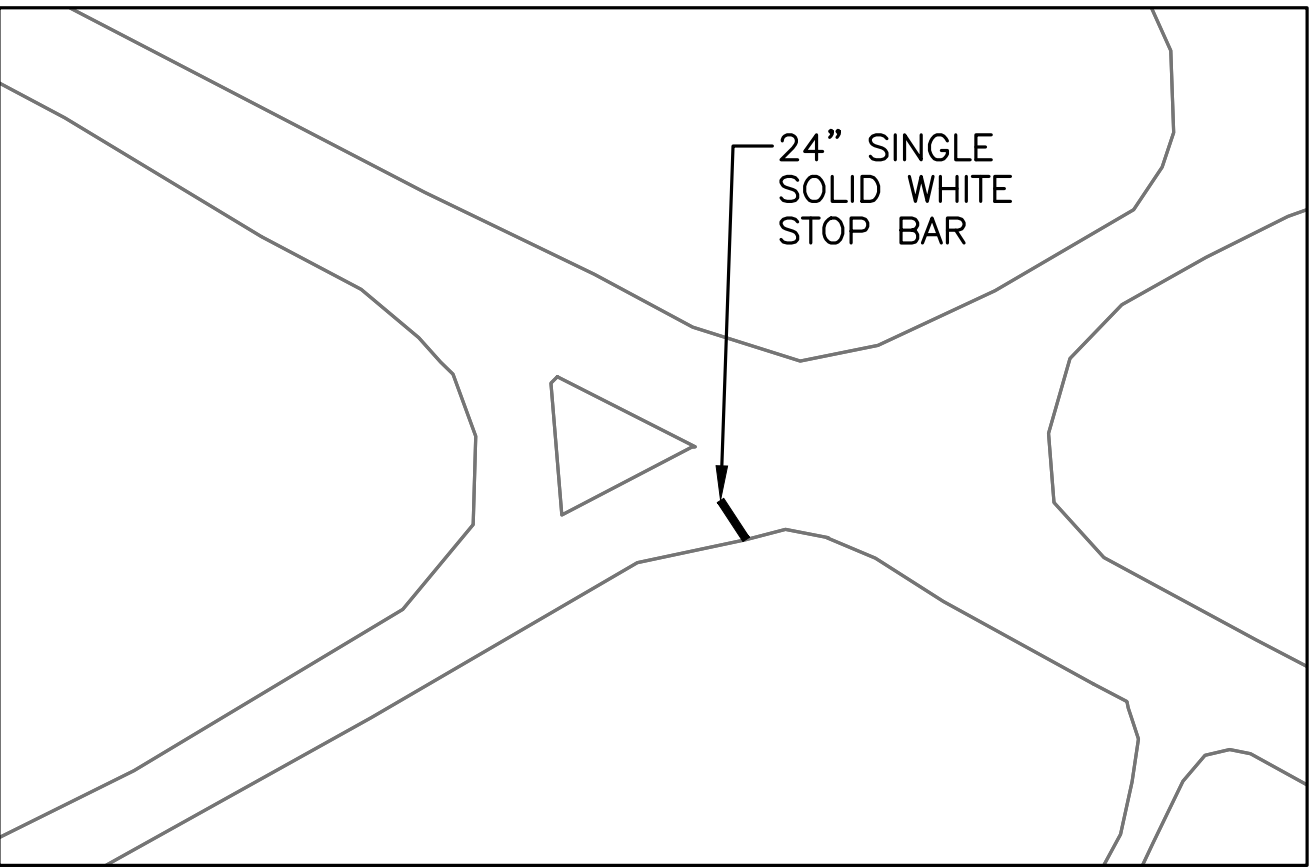
**NOTES:**

1. 2" OVERLAY OF SM-9.5A IS REQUIRED FOR ENTIRE PAVEMENT AREA. PRIOR TO OVERLAY, LOCALIZED PAVEMENT REPAIRS SHALL BE COMPLETED IN ACCORDANCE WITH PATCHING DETAILS.
2. PRIOR TO MCCLAWS DRIVE REPAIRS, CONTRACTOR SHALL DOCUMENT EXISTING STRIPING PATTERNS. UPON COMPLETION OF OVERLAY, STRIPING PATTERNS SHALL BE RESTORED.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
4. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.

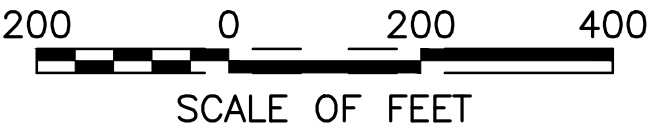
LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT



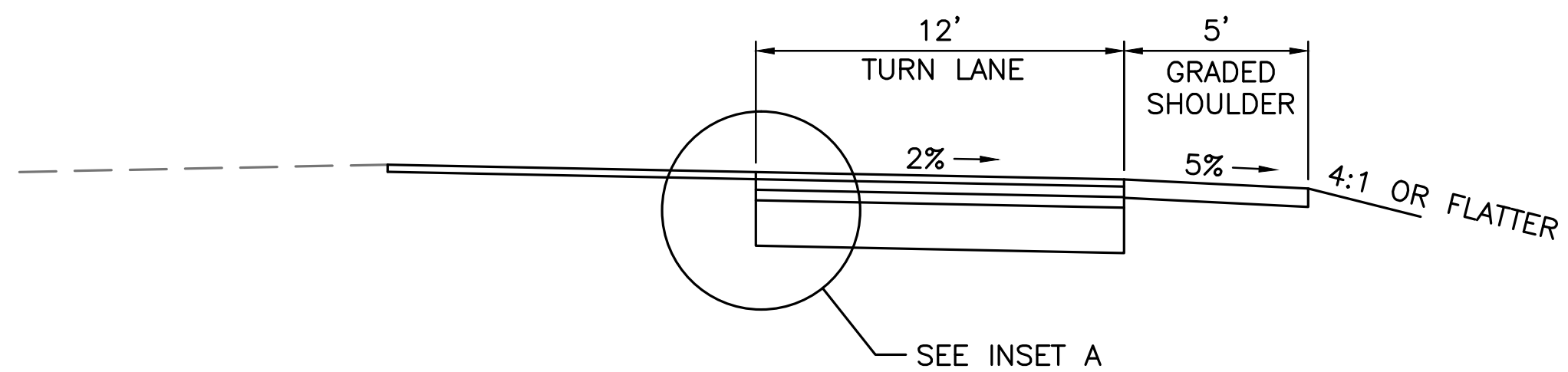
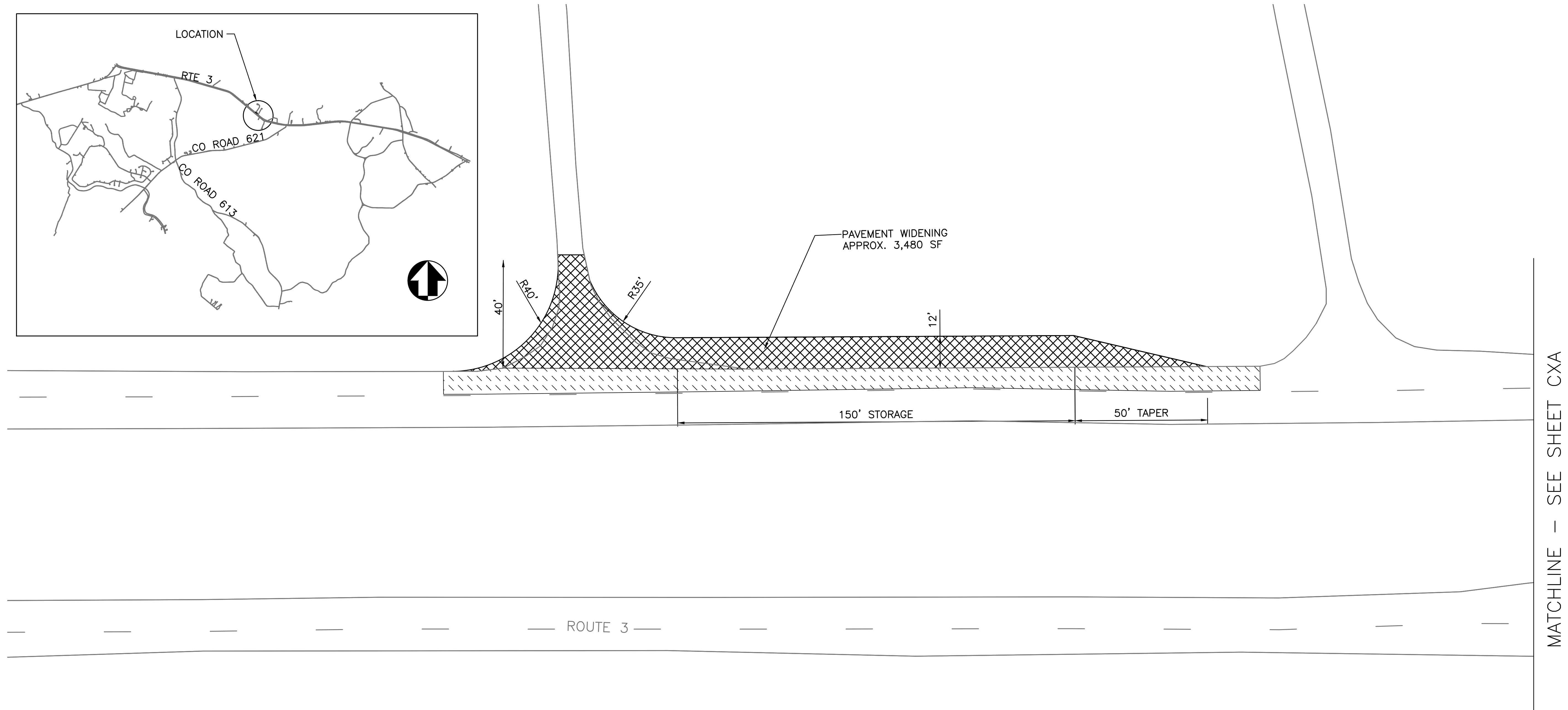
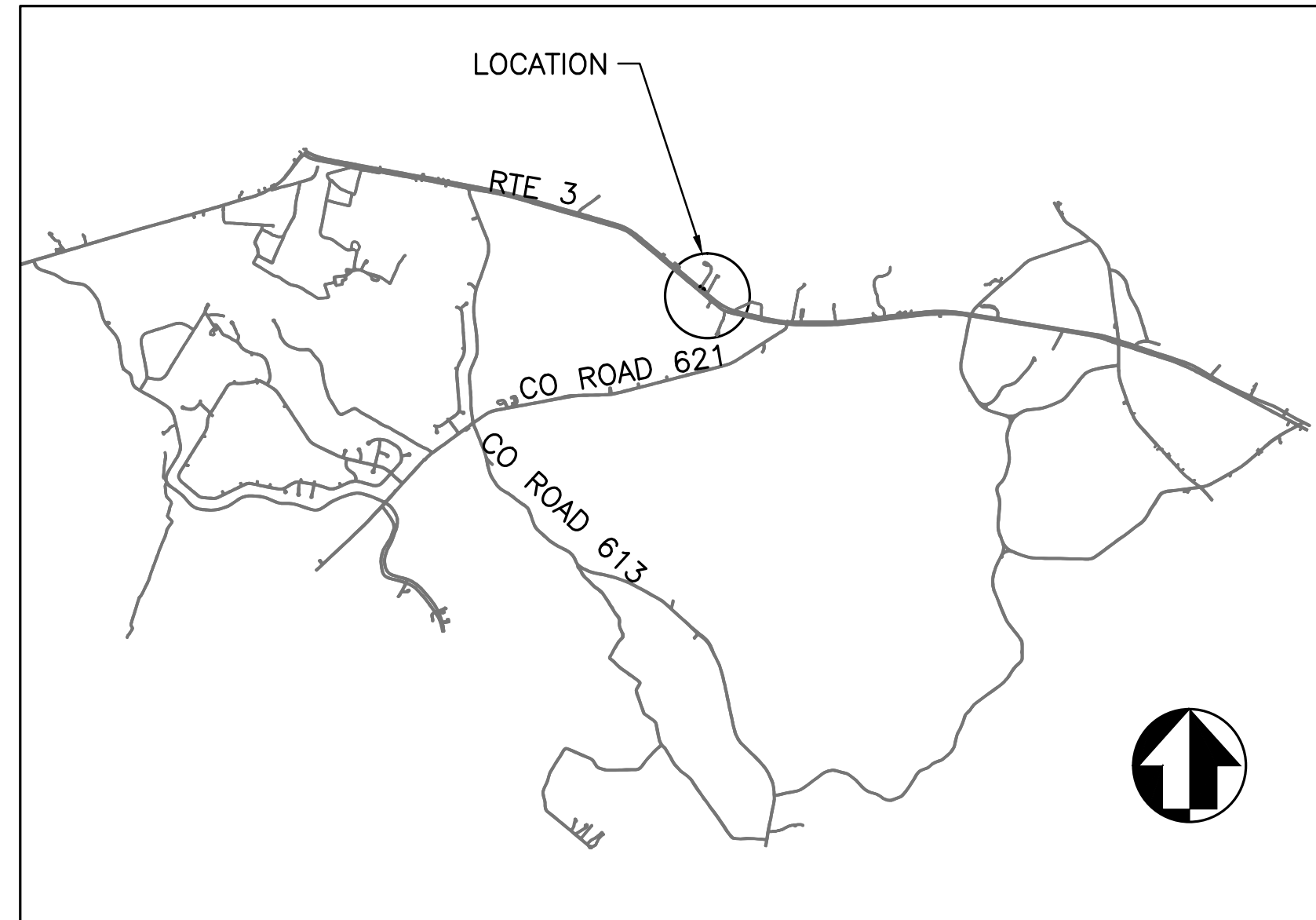
INSET A



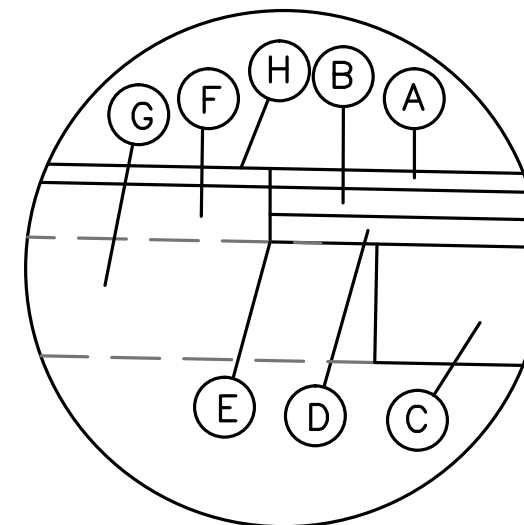
INSET B



DESIGNED:	SUB SHEET NO.	TITLE OF SHEET		DRAWING NO.
CAL	C13	MCCLAWS DRIVE		_____
ALS		ROUTE 13		PMIS/PKG NO.
TECH. REVIEW:		FREDERICKSBURG AND SPOTSYLVANIA		147784
TNR		NATIONAL MILITARY PARK		SHEET
DATE:	03-11-11			19 OF 23



TURN LANE SECTION



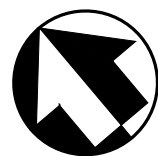
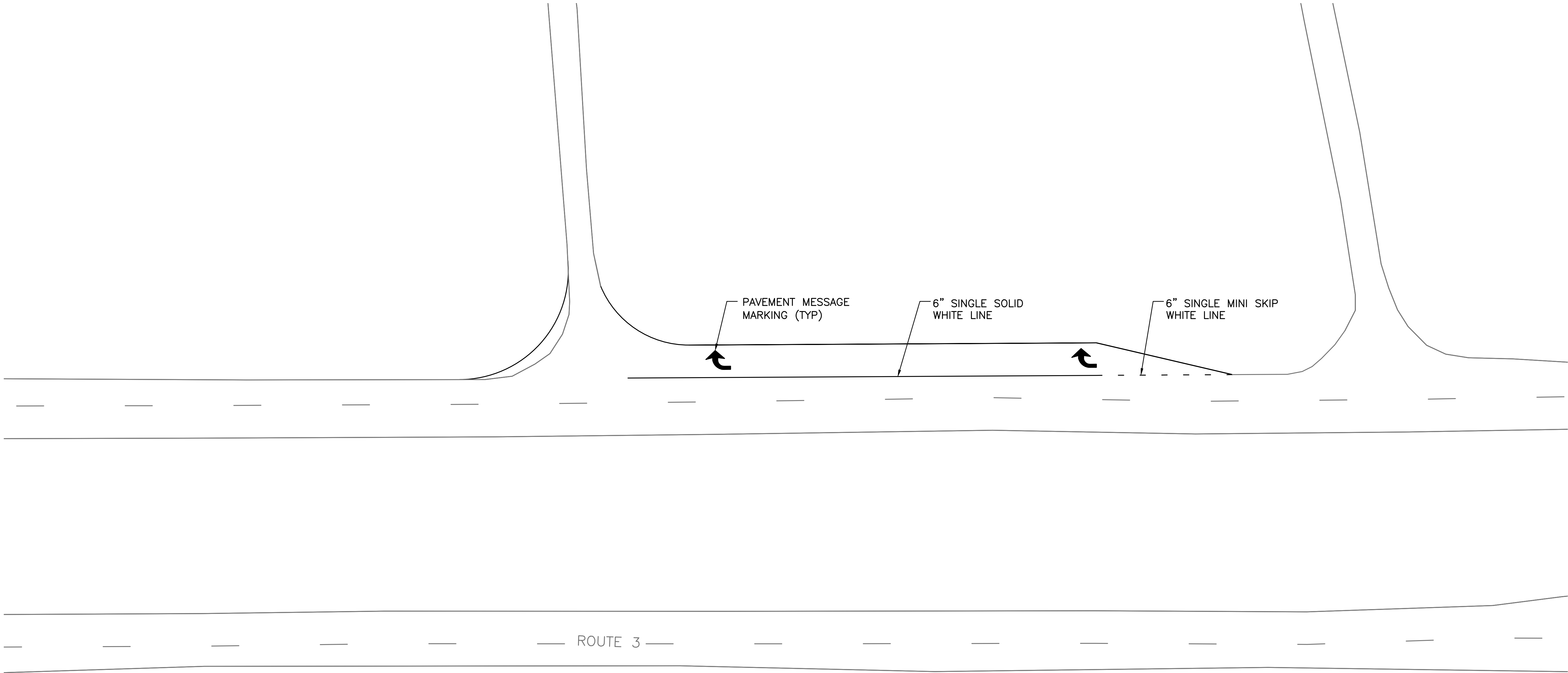
INSET A

- (A) 2" ASPHALT CONCRETE, SM-9.5A
- (B) 2" ASPHALT CONCRETE, IM-19.0A
- (C) 4" ASPHALT CONCRETE, BM-25.0A
- (D) 10" PLAIN AGGREGATE, TYPE 1, SIZE NO 21A
- (E) FULL DEPTH SAWCUT 1' FROM PROPOSED EDGE  
OF PAVEMENT
- (F) EXISTING ASPHALT PAVEMENT
- (G) EXISTING SUBBASE
- (H) 2" MILL AND OVERLAY (SM-9.5A)

DESIGNED:	SUB SHEET NO.	TITLE OF SHEET	DRAWING NO.
CAL	C14	JACKSON'S FLANK ATTACK TURN LANE	_____
<b>CADD</b>			PMIS/PKG NO. 147784
ALS			SHEET 20 OF 23
TECH. REVIEW:			
TNR			
DATE: 03-11-11		FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	

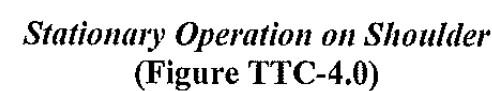


3/10/11 09:09 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\C144-JACKSONSELANK-PWMTMRK.DWG  
XREFS: \\Vib\proj\VaBeach\33400.03 FRSP Pavement Rehab\cad\FRSP\147784\Drawings\bases\DATE.dwg\XC-MATCHLINE.dwg\\Vib\proj\VaBeach\33400.03 FRSP Pavement Rehab\cad\FRSP\147784\Drawings\bases\XC-PROP-PAVEMRK.dwg\\Vib\proj\VaBeach\33400.03 FRSP Pavement Rehab\cad\FRSP\147784\...



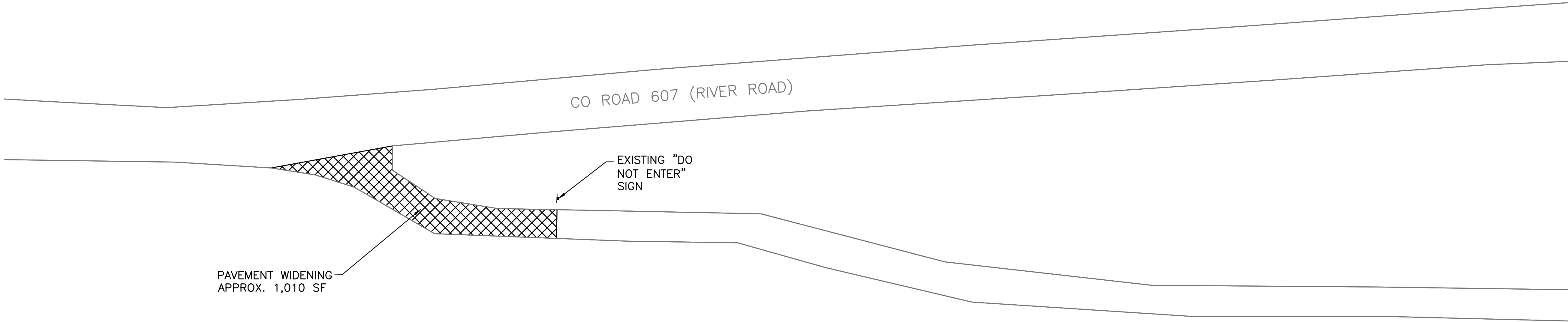
DESIGNED: CAL	SUB SHEET NO.  <b>C14A</b>	TITLE OF SHEET <b>JACKSON'S FLANK ATTACK TURN LANE PAVMENT MARKING PLAN</b> FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO. _____
TECH. REVIEW: TNR			PMIS/PKG NO. 147784
DATE: 03-11-11			SHEET <b>21</b> of <b>23</b>

9. When a side road intersects the highway within the temporary traffic control zone, additional traffic control devices shall be placed as needed.



DESIGNED:	SUB SHEET NO.  <b>C14B</b>	TITLE OF SHEET	DRAWING NO.
CAL		<b>JACKSON'S FLANK ATTACK MAINTENANCE OF TRAFFIC DETAILS</b>	_____
<b>CADD</b>			PMIS/PKG NO. 147784
ALS			SHEET <b>22 OF 23</b>
TECH. REVIEW:			
TNR		FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	
DATE: 03-11-11			

3/10/11 09:20 ASCHAKE R18 P:\33400.03 FRSP PAVEMENT REHAB\CAD\FRSP\147784\DRAWINGS\CIVIL\C15-DRIVEWAY.DWG  
XREFS: \\vib\pro\VoBeach\33400.03 FRSP Pavement Rehab\cod\FRSP\147784\Drawings\bases\DATE.dwg\XC-MATCHLINE.dwg\XC-PROF-ROAD.dwg\XC-Vib\pro\VoBeach\33400.03 FRSP Pavement Rehab\cod\FRSP\147784\Drawings\bases\XC-SITE.dwg



NOTES:

1. FULL DEPTH PAVEMENT SECTION REQUIRES 12" DEEP EXCAVATION AND REMOVAL OF EXISTING SURFACE PRIOR TO PLACING WOVEN FABRIC ON THE SUBGRADE WITH 3" SM-9.5A IN TWO EQUAL LIFTS ON 9" AGGREGATE 21A.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING EXISTING UTILITY COVERS INCLUDING, BUT NOT LIMITED TO, UTILITY MANHOLES (EXCLUDING DRAINAGE EXCEPT AS NOTED), PULL BOXES AND VALVES TO PROPOSED GRADE.
3. FOR TYPICAL MAINTENANCE OF TRAFFIC DETAILS SEE SHEETS C3 AND C3A.

LEGEND	
	SM 9.5A HMA OVERLAY
	FULL DEPTH PAVEMENT



DESIGNED:	SUB SHEET NO.  <div>C15</div>	TITLE OF SHEET  CHATHAM MANOR DRIVEWAY  FREDERICKSBURG AND SPOTSYLVANIA NATIONAL MILITARY PARK	DRAWING NO.  _____
CAL			PMIS/PKG NO. 147784  SHEET <u>23</u> OF <u>23</u>
<b>CADD</b>			
ALS			
TECH. REVIEW:			
TNR			
DATE: 03-11-11			